SHEET INDEX

SHEET **NUMBER**

SHEET NAME

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THIS UR PLAN IDENTIFIES EXISTING UTILITY RIGHT-OF-WAY, ADDITIONAL UTILITY RIGHTS-OF-WAY MAY NEED TO BE ESTABLISHED IN THE COURSE OF IMPLEMENTING THIS UR, FOLLOWING FINAL DESIGN. IN SUCH EVENT, ALL SUCH PROPOSED UTILITY RIGHTS-OF-WAY SHALL BE IDENTIFIED ON THE FINAL PLAT SUBMITTAL.

ALL PROPOSED BUILDING ABOVE 5 STORIES IN HEIGHT MUST BE APPROVED BY AVIATION DEPARTMENT PRIOR TO FILING FOR ANY APPLICATION WITH THE CITY

CONCEPT PLANS ARE PROVIDED WITH THE ANTICIPATION OF SUBSEQUENT PROJECT PLAN APPROVAL FOR EACH PARCEL PRIOR TO CONSTRUCTION. EACH NEW BUILDING WILL FOLLOW THE DESIGN GUIDELINES SET FOR IN THE GREATER DOWNTOWN AREA PLAN DEVELOPMENT GUIDELINES

PHASING OUTLINE

PHASE 1 CONSISTS OF THE FOLLOWING:

- FINAL PLATTING OF BLOCKS B/1 AND B/3
- DEMOLITION OF WELD WHEEL 9 STORY STRUCTURE, BLOCK B/1 AND 2 STORY BUILDING ON BLOCK D/2
- VACATION OF ST. LOUIS AVE BETWEEN MULBERRY AND SANTA FE VACATION OF UTILITY EASEMENT BETWEEN BLOCK B/3 AND B/1
- VACATION OF ALLEY BETWEEN BLOCK B/1 AND RAILROAD PROPERTY NORTH OF ST LOUIS
- VACATION OF ARIEL EASEMENT NORTH OF BLOCK C/1
- VACATION OF EASEMENT UNDER FORESTER OVERPASS
- VACATION OF FAULTLESS ACCESS EASEMENT DEVELOPMENT OF BLOCKS A/1, C/1,C/2, F/1, AND A PORTION OF BLOCK D/2 INTO SURFACE PARKING
- ADAPTIVE REUSE DEVELOPMENT OF BLOCKS B/2, B/3, E/1, E/2, E/3, E/4, H/2
- DEVELOPMENT OF BLOCK B/1
- PUBLIC IMPROVEMENTS ALONG UNION AVE FROM MULBERRY ST TO SANTA FE ST, ON SANTA FE ST FROM 12TH ST TO ST LOUIS AVE, 11TH FROM MULBERRY ST TO SANTA FE AND, MULBERRY BETWEEN 11TH ST NORTH TO THE ALLEY EASEMENT

PHASE 2 CONSISTS OF THE FOLLOWING:

- DEMOLITION OF SINGLE AND TWO STORY STRUCTURES, BLOCK D/1 AND D/2
- FINAL PLATTING OF BLOCKS D/1 AND D/2 DEVELOPMENT OF BLOCKS D/1 AND D/2
- PUBLIC IMPROVEMENTS ALONG UNION AVE FROM MULBERRY ST TO HICKORY ST, 11TH ST TO HICKORY ST, AND MULBERRY BETWEEN UNION SOUTH TO ALLEY EASEMENT

PHASE 3 CONSISTS OF THE FOLLOWING:

- VACATION OF ALLEY BETWEEN BLOCK G/1 AND G/2
- ADAPTIVE REUSE DEVELOPMENT OF BLOCK G/1
- DEVELOPMENT OF BLOCK G/3
- PUBLIC IMPROVEMENTS ALONG 12TH ST FROM HICKORY ST TO MULBERRY ST

PHASE 4 CONSISTS OF THE FOLLOWING:

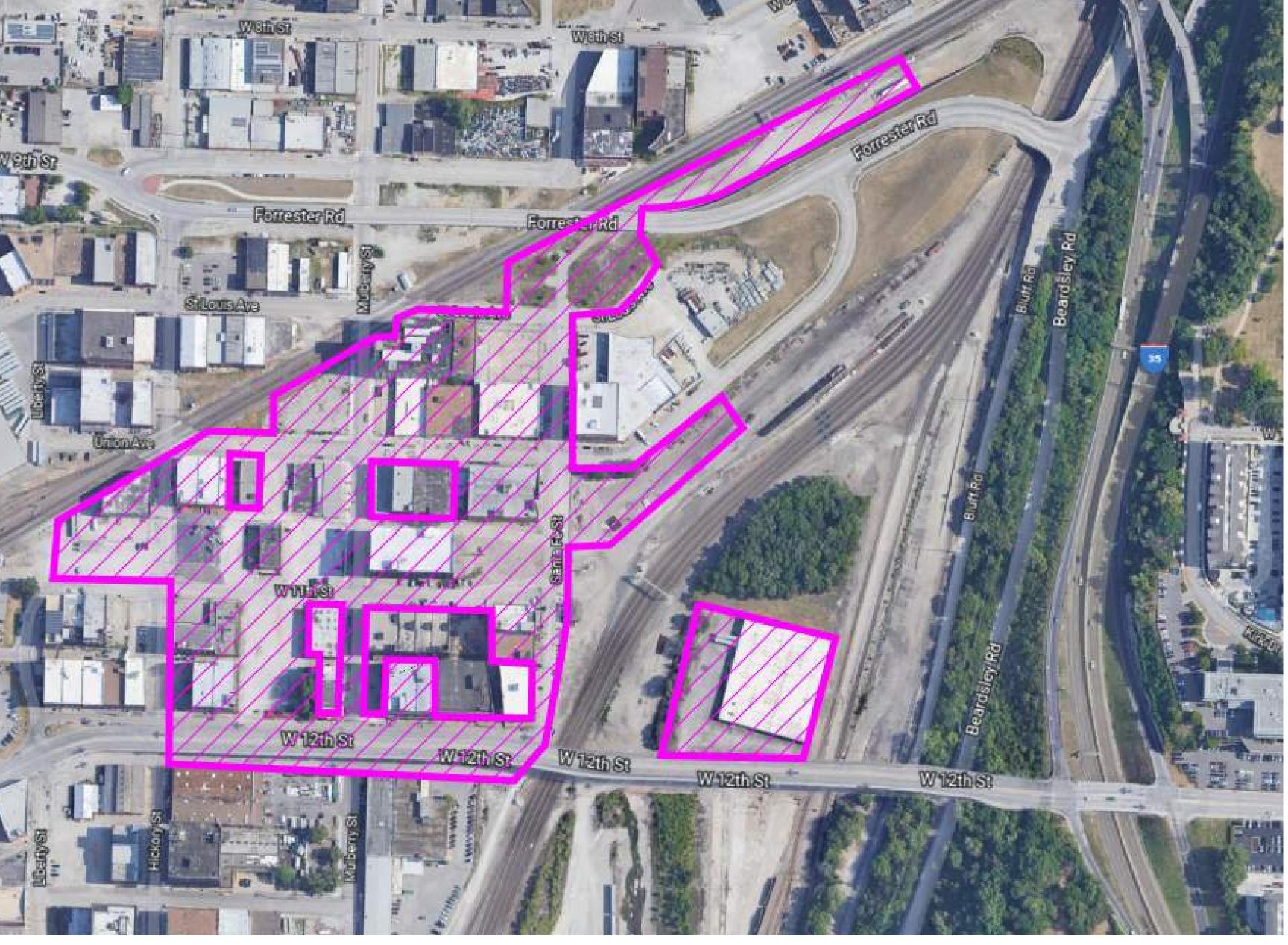
- DEMOLITION OF 2 STORY STRUCTURE ON BLOCK J/1
- DEVELOPMENT OF BLOCKS AND A/1, C/1, AND J/1
- ADAPTIVE REUSE DEVELOPMENT OF BLOCK G/2, H/1, AND I/1
- PUBLIC IMPROVEMENTS ALONG 12TH ST BETWEEN HICKORY

PHASE 5 CONSISTS OF THE FOLLOWING:

- ADAPTIVE REUSE DEVELOPMENT OF BLOCK D/3, D/4, J/1, J/2
- PUBLIC IMPROVEMENTS ALONG HICKORY ST BETWEEN 11TH STAND UNION AVE

PARKLAND DEDICATION WILL BE SATISFIED THROUGH A COMBINATION OF CREDITS FOR ALLOCATED OPEN SPACE AND / OR RECREATIONAL SPACE AND MONEY -IN-LIEU OF PARKLAND DEDICATION. THE TOTAL AREA THAT WILL BE DEDICATED TO THE PUBLIC RECREATION HAS NOT BEEN ESTABLISHED AND WILL BE DETERMINED IN A PHASE-BY-PHASE TIMELINE AS PROJECT DESIGNS ARE COMPLETED.

ALL IMPROVEMENTS TO STREETSCAPE, SIDEWALKS, PLAZAS AND OPEN SPACE AMENITY AREAS WILL BE DESIGNED IN ACCORDANCE WITH THE RECOMMENDATION OF THE WEST BOTTOMS STREETSCAPE AND WAYFINDING PLAN







WEST BOTTOMS REDEVELOPMENT UR DISTRICT AND PRELIMINARY PLAT

SomeraRoad

West Bottoms, Kansas City, MO

Somera Road Kansas City, MO 64101



300 West 22nd Street Kansas City, MO 64108 USA t +1 816 472 3360 f +1 816 472 2100

4600 COLLEGE BLVD SUITE 10

NEW YORK, NY 10001 ASSOCIATE ARCHITECT BKV GROUP 222 NORTH 2ND STREET, SUITE 101 MINNEAPOLIS, MN 55401

MEP ENGINEER LANKFORD FENDLER + ASSOCIATES 1730 WALNUT KANSAS CITY, MO 64108

UR REZONING INFORMATION (28)

Block / Lot #	Building name	A. EX/PRE. ZONING	B. LAND AREA (AC.)	B. LAND AREA (SF.)	D. NET LAND AREA	E. USE***	F. HEIGHT ABOVE GRADE/ FLOORS	G.GROSS AREA TOTAL	G.GROSS FLOOR AREA PER FLOOR (ADVERAGE)	H. COVERAGE/ FLOOR AREA RATIO	I. RESIDEINTALS UNITS	I. RESIDEINTAL NET DENSITY (DU/Ac)	J. REQUIRED PARKING^	J. PARKING PROVIDED	BIKE PARKING PROVIDED LONG TERM	BIKE PARKING PROVIDED SHORT TERM	PHASE	COMMENCE & COMPLTETE DATES
A/1*	D	UR / UR	0.43	18,734	17,250	PARKING		18,734				2 U		41			EXISTING	Q1.2023 - Q4.2025
A / 1*	D	UR / UR	0.43	18,734	17,241	MIXED USE/RESIDENTAL	150'/15	74,129	14,826	3.96	72	167	39	23	14	3	PHASE 4	Q4.2029-Q4-2030
B /1	E/F	UR / UR	2.05	89,170	76,650	MIXED USE/RESIDENTAL	150'/15	132,592	26,518	1.49	266	130	152	138	53	14	PHASE 1	Q1.2023 - Q4.2025
B / 2	WELD WHEEL	UR / UR	0.25	10,794	6,863	MIXED USE	75' / 2	13,726	13,726	1.27			21	23	11	3	EXISTING/PHASE 1	Q1.2023 - Q4.2025
B/3	PSB	UR / UR	0.45	19,487	16,418	MIXED USE	55' / 3	63,826	15,957	0.82			96		49	3	EXISTING/PHASE 1	Q1.2023 - Q4.2025
C/1*	G	DX-10 / UR	0.41	17,863	20,386	PARKING		74,129						46			PHASE 1	Q1.2023 - Q4.2025
C / 1*	G	DX-10 / UR	0.58	25,180	20,386	MIXED USE/RESIDENTAL	150'/15	74,129	12,355	2.94	72	125	36	23	14	3	PHASE 4	Q4.2029-Q4-2030
C / 2	FAULTLESS	M3-5 / UR	0.66	28,591	28,551	PARKING		28,591						46			PHASE 1	Q1.2023 - Q4.2025
D/1	В	M3-5 / UR	0.26	11,473	28,713	MIXED USE/RESIDENTAL	150'/15	82,433	16,487	7.18	148	562	88		30	3	PHASE 2	Q4.2024-Q4.2026
D/2*	Α	M3-5 / UR	1.06	46,345	42,547	PARKING		42,547						200			EXISTING	Q1.2023 - Q4.2025
D / 2*	Α	M3-5 / UR	1.06	46,345	42,547	MIXED USE/RESIDENTAL	150'/15	254,032	42,339	5.48	160	150	94	379	32	38	PHASE 2	Q4.2024-Q4.2026
D/3	NATHAN	M3-5 / UR	0.26	11,473	11,552	MIXED USE/RESIDENTAL	55'/4	46,208	11,552	4.03	43	164	33		9	3	EXISTING/PHASE 5	Q1.2030 - Q4.2035
D/4	1323	M3-5 / UR	0.07	2,897	2,888	MIXED USE/RESIDENTAL	45'/3	8,664	2,888	2.99	11	163	8		2	3	EXISTING/PHASE 5	Q1.2030 - Q4.2035
D/5	1317	M3-5 / UR	0.13	5,808	4,835	MIXED USE/RESIDENTAL	31'/2	9,670	4,835	1.66	12	91	9		2	3	EXISTING/PHASE 2	Q4.2024-Q4.2026
E/1	FIREHOUSE	M3-5 / UR	0.08	3,615	1,943	MIXED USE	31'/ 2	3,352	2,136	0.93			3		2	3	EXISTING/PHASE 1	Q1.2023 - Q4.2025
E / 2	LARMIE	M3-5 / UR	0.45	19,583	19,430	MIXED USE	80' / 5	116,580	19,578	5.95			175		88	3	EXISTING/PHASE 1	Q1.2023 - Q4.2025
E /3	MOLINE	M3-5 / UR	0.52	22,810	19,242	MIXED USE/RESIDENTAL	120' / 7	159,670	19,959	7.00	127	243	73		25	3	EXISTING/PHASE 1	Q1.2023 - Q4.2025
E /4**	TOWN CENTER	M3-5 / UR	0.39	17,119	OPEN	PARK		17,119									EXISTING/PHASE 1	Q1.2023 - Q4.2025
F/1	SANTA FE PARKING	/ UR	1.20	52,119	53,664	PARKING		53,664						187			EXISTING/PHASE 1	Q1.2023 - Q4.2025
G/1	CRANE	M3-5 / UR	0.41	17,976	5,863	MIXED USE/RESIDENTAL	92' / 6	35,178	5,871	1.96	31	75	21	22	6	3	EXISTING/PHASE 3	Q4.2026 - Q4.2028
G / 2	BOLT-NUT	M3-5 / UR	0.41	18,004	10,230	MIXED USE/RESIDENTAL	90' / 5	51,150	10,230	2.84	45	108	33	14	9	3	EXISTING/PHASE 4	Q4.2029 - Q4.2030
G / 3	С	M3-5 / UR	1.03	44,994	44,994	MIXED USE/RESIDENTAL	150'/15	258,225	17,215	5.74	236	228	126	184	47	18	PHASE 3	Q4.2026 - Q4.2028
H/1	ADVANCE	M3-5 / UR	0.32	13,851	13,797	MIXED USE	110' / 6	82,782	13,678	5.98			124		63		EXISTING/PHASE 1	Q1.2023 - Q4.2025
H / 2	AVERY	M3-5 / UR	0.13	5,770	5,663	HOTEL^^	120' / 7	39,641	5,663	6.87			15	30	3	3	EXISTING/PHASE 4	Q4.2029 - Q4.2030
1/1	1000 W 12TH	M3-5 / UR	3.09	134,600	83,710	MIXED USE	18' / 1	50,890	50,890	0.38			76	103	39	10	EXISTING/PHASE 4	Q4.2029 - Q4.2030
J/1	CONVIVIAL	M3-5 / UR	0.18	7,847	7,715	MIXED USE/RESIDENTAL	55'/4	30,860	7,715	3.93	27		21		24	3	EXISTING/PHASE 5	Q1.2030 - Q4.2035
J/2	LOT	M3-5 / UR	0.67	29,256	29,296	PARKING		29,256						70			EXISTING/PHASE 5	Q1.2030 - Q4.2035
	7.512	^^^GROSS AREA	21.85		-	· · · · · · · · · · · · · · · · · · ·	4			TOTAL UNITS	1250	TOTAL	1242	1242	524	125		
	•	· · · · · · · · · · · · · · · · · · ·								GROSS DENSITY	57.20							

^{*}Parcels are 2 phases, re: phasing plan

No. Description

NOT FOR CONSTRUCTION

	Boompton	Batt
001	UR SUBMITTAL	05.02.20
002	UR RE-SUBMITTAL	06.06.20

VICINITY MAP, PHASING NARRATIVE AND UR REZONING INFORMATION

Original is $48\,x\,36$. Do not scale contents of this drawing. Sheet Number

^{**} Does not include the ROW on 11th

^{***}Use and density may be interchanged among the parcels

[^]Residential Parking required for existing building is base on unit count + 50% Net Land Area mixed use

[^]Residential Parking required for new building is base on unit count + 25% Net Land Area mixed use

^{^^}based on 50 keys

^{^^^}includes area in RoW

Use and density may be interchanged among the parcels

SIGNAGE PLAN (KCMO Zoning & Development code 88-445 Signs)

Block / Lot #	EX/PRE. ZONING	USE	HEIGHT ABOVE GRADE/ FLOORS	Monument Sign 88-445-08.A	Oversized Monument Sign 88-445-08.B	Wall Sign 88-445-08.C	Awning, Canopy, Large Marquee Under Marquee Sign 88-445-08.D	Projecting Signs (Blade) 88-445-08.E	Roof Signs 88-445-08.F	Site Signage
A/1	UR/UR	PARKING						1		
A/1	UR/UR	MIXED USE/RESIDENTAL	84' / 5			4		1		
B/1	UR/UR	MIXED USE/RESIDENTAL	78' / 5			6		2		
B/2	UR/UR	MIXED USE	75' / 2			4		4		
B/3	UR/UR	MIXED USE	55'/3			3		2	1	
C/1	DX-10 / UR	PARKING						2		
C/1	DX-10/UR	MIXED USE/RESIDENTAL	75' / 6			2				
C/2	M3-5 / UR	PARKING						2		
D/1	M3-5 / UR	MIXED USE/RESIDENTAL	90' / 5			2				
D/2*	M3-5 / UR	PARKING						2		
D/2*	M3-5 / UR	MIXED USE/RESIDENTAL	90' / 6			3		1		
D/3	M3-5 / UR	MIXED USE/RESIDENTAL	55' / 4			6		6		
D/4	M3-5 / UR	MIXED USE/RESIDENTAL	45'/3		-	2		2		
E/1	M3-5 / UR	RESTAURANT	31/2			2		1		
E/2	M3-5 / UR	MIXED USE	80' / 5			5	2	6		
E/3	M3-5 / UR	MIXED USE/RESIDENTAL	120'/7			10		8		
E/4	M3-5 / UR	PARK			1					
F/1	/UR	PARKING								2
G/1	M3-5 / UR	MIXED USE/RESIDENTAL	92' / 6			2				
G/2	M3-5 / UR	MIXED USE/RESIDENTAL	90' / 5			2				
G/3	M3-5 / UR	MIXED USE/RESIDENTAL	95' / 7			2				
H/1	M3-5 / UR	MIXED USE/RESIDENTAL	110'/6			2				
H/2	M3-5 / UR	HOTEL	120'/7			3	1	1		
1/1	M3-5 / UR	MIXED USE	18'/1	1		3		1		
1/2	/ UR	PARK								2
J/1	M3-5 / UR	MIXED USE/RESIDENTAL	55'/3			4		3		
J/2	M3-5 / UR	MIXED USE/RESIDENTAL								2
Entire	Site	OFF-SITE DISTRICT			0					6
Littire	5 Oile	IN-DISTRICT								3

Directional Messaging to be implemented on the Existin Out-of-District Signs and provided NEW Out-of-District wayfinding and district welcome signs per current city-wide wayfinding

J. PURPOSED PARKING

88-420-02-C. CHANGE OF USE OR OCCUPANCY:

UNLESS OTHERWISE EXPRESSLY STATED, WHEN THE USE OR OCCUPANCY OF PROPERTY CHANGES, ADDITIONAL OFF-STREET PARKING AND LOADING FACILITIES MUST BE PROVIDED TO SERVE THE NEW USE OR OCCUPANCY ONLY WHEN THE NUMBER OF PARKING OR LOADING SPACES REQUIRED FOR THE USE OF OCCUPANCY EXCEEDS THE NUMBER OF SPACES REQUIRED FOR THE USE THAT MOST RECENT OCCUPIED THE BUILDING BASED ON THE STANDARDS OF THIS ZONING AND DEVELOPMENT CODE. IN OTHER WORDS, "CREDIT" IS GIVEN TO THE MOST RECENT LAWFUL USE OF THE PROPERTY FOR THE NUMBER OF PARKING SPACES THAT WOULD BE REQUIRED UNDER THE ZONING AND DEVELOPMENT CODE, REGARDLESS OF WHETHER SUCH SPACES ARE ACTUALLY PROVIDED. A NEW NONRESIDENTIAL USE IS NOT REQUIRED TO ADDRESS A LAWFUL, EXISTING PARKING DEFICIT

NOTE: THE DEVELOPMENT WILL BE RECOGNIZED AS A HISTORIC DISTRICT.

88-420-04-H. LANDMARKS AND HISTORIC DISTRICT:

- 1. NO OFF-STREET PARKING OR LOADING SPACED ARE REQUIRED FOR REHABILITATION OR REUSE OF AN OFFICIAL LOCAL OR NATIONALLY HISTRIONIC DISTRICT
- 2. NO OFF-STREET PARKING SPACES ARE REQUIRED FOR REHABILITATE OR REUSE OF AN EXISTING CONTRIBUTING BUILDING WITHIN AN OFFICIAL LOCAL OR NATIONAL HISTORIC DISTRICT

UR VEHICLE PARKING RATIOS

RESIDENTIAL - .5 PER UNIT

MIXED USE - 2 PER 1K

RESTAURANT - 2 PER 1K SF

HOTEL - .3 PER KEY

EHICLE PARKING	PHASE 1	PHASE 2	PHASE 3	PHASE 4	PHASE 5	TOTAL
REQUIRED	643	191	147	199	62	1242
PROVIDED	681	379	206	193	70	1242*

*227 ON-STREET PARKING STALLS NOT INCLUDED IN TOTAL

UR BICYCLE PARKING RATIOS LONG TERM BICYCLE PARKING -

- RESIDENTIAL 1 PER 5 DWELLING UNITS
- MIXED USE 1 + 1PER 10K SF
- RESTAURANT 1 + 1 PER 5K SF
- HOTEL 1 + 1PER 30 ROOMS

SHORT TERM BICYCLE PARKING - RESIDENTIAL/ NONRESIDENTIAL - 3 OR EQUAL NUMBER TO AT LEAST 10% OF THE ACTUAL NUMBER – WHICH EVER ONE IS GREATER

BICYCLE PARKING REQUIRED NUMBER

OF SPACES	PHASE 1	PHASE 2	PHASE 3	PHASE 4	PHASE 5	TOTAL
SHORT TERM BICYCLE PARKING	292	64	53	80	35	524
LONG TERM BICYCLE PARKING	29	44	21	22	9	125

PROHIBITED USE GROUPS

EACH BUILDING PROJECT WILL CONFORM TO THE PERMITTED USE AS ALLOWED BY DX ZONING AND WILL BE SUBMITTED FOR FINAL PLANS APPROVAL PRIOR TO CONSTRUCTION.

PROPERTIES WITHIN THE UR DISTRICT ARE SUBJECT TO THE USE REGULATIONS AND LOT AND BUILDING STANDARDS ESTABLISHED AT THE TIME OF PRELIMINARY DEVELOPMENT PLAN APPROVAL BY THE CITY COUNCIL. THE USE AND DEVELOPMENT STANDARDS ESTABLISHED FOR THE SUBJECT UR DISTRICT MUST BE IN GENERAL CONFORMANCE WITH THE APPROVED AREA PLAN AND BE COMPATIBLE WITH DESIRABLE LAND USE AND DEVELOPMENT PATTERNS IN THE SURROUNDING AREA.

RESIDENTIAL

- **GROUP LIVING (AS LISTED)**
- DOMESTIC VIOLENCE RESIDENCE
- DOMESTIC VIOLENCE SHELTER
- GROUP HOMES
- NURSING HOMES (UNASSOCIATED WITH ASSISTED LIVING FACILITIES)

PUBLIC/CIVIC

DAY CARE (AS LISTED)

 HOME BASED FAMILY (6 AND UP) **DETENTION AND CORRECTIONAL FACILITIES - ALL**

HALFWAY HOUSE - ALL **SAFETY SERVICE** (AS LISTED)

- AMBULANCE
- FIRE STATION
- POLICE STATION (WITH MULTI-VEHICULAR RESPONSE SERVICES)

COMMERCIAL

ADULT BUSINESS - ALL **ANIMAL** (AS LISTED)

STABLE

BUILDING EQUIPMENT SALES AND SERVICE - ALL

- **CONVENIENCE STORE** (NON NEIGHBORHOOD SERVING) (AS LISTED) WITH FUELING STATION
- OFFERING THE SALE OF MATERIALS AND EQUIPMENT TO BE USED IN CONSUMING OR PREPARING ILLEGAL SUBSTANCES

DAY LABOR EMPLOYMENT AGENCY - ALL FUNERAL AND INTERNMENT SERVICES - ALL **GASOLINE AND FUEL SALES** - ALL **HOOKAH BAR** - ALL

HOSPITAL (FULL-SERVICE) - ALL **LODGING (AS LISTED)**

 MANUFACTURED HOME PARKS RECREATIONAL VEHICLE PARK

MOTION PICTURE ARCADE BOOTH - ALL OFFICE, MEDICAL (AS LISTED) BLOOD/PLASMA CENTER

PAWN SHOP - ALL **RETAIL AND WHOLESALE SALES (AS LISTED)**

AUTOMOTIVE PARTS

FIREARMS AND WEAPONS

SHELTER - ALL SHORT-TERM LOAN ESTABLISHMENT - ALL

TRANSPORTATION (AS LISTED) MAINTENANCE FACILITIES

VEHICLE SALES AND SERVICE (AS LISTED)

- CAR WASH/CLEANING SERVICE (FREE-STANDING)
- HEAVY EQUIPMENT SALES/RENTAL MOTOR VEHICLE REPAIR, GENERAL
- VEHICLE STORAGE/TOWING

INDUSTRIAL AUTO WRECKING - ALL **JUNK/SALVAGE YARD** - ALL MINING AND QUARRYING - ALL **RECYCLING SERVICE - ALL**

WASTE-RELATED USE - ALL

AGRICULTURE ANIMAL - ALL

WIRELESS COMMUNICATION FACILITY (AS LISTED)

FREESTANDING FACILITY

NOT FOR CONSTRUCTION

SomeraRoad

WEST BOTTOMS

DISTRICT AND

REDEVELOPMENT UR

PRELIMINARY PLAT

West Bottoms, Kansas City, MO

Prepared For

Somera Road

300 West 22nd Street

Consultant Name

SK DESIGN GROUP, LNC.

4600 COLLEGE BLVD SUITE 10

OVERLAND PARK, KS 6621

MASTER PLAN ARCHITECT

322 8TH AVENUE

MEP ENGINEER

NEW YORK, NY 10001

ASSOCIATE ARCHITECT BKV GROUP

MINNEAPOLIS. MN 55401

KANSAS CITY, MO 64108

222 NORTH 2ND STREET, SUITE 101

LANKFORD FENDLER + ASSOCIATES

Kansas City, MO 64108 USA

t +1 816 472 3360 f +1 816 472 2100

Kansas City, MO 64101

No. Description

SINAGE PLAN, PARKING, PROHIBITED USE, AND LEGAL BOUNDARY

DESCRIPTION

G001

.. BOUNDARY DESCRIPTION

DESCRIPTION

OF BEGINNING.

CONTAINING IN ALL 21.85 ACRES OF LAND MORE OR LESS.

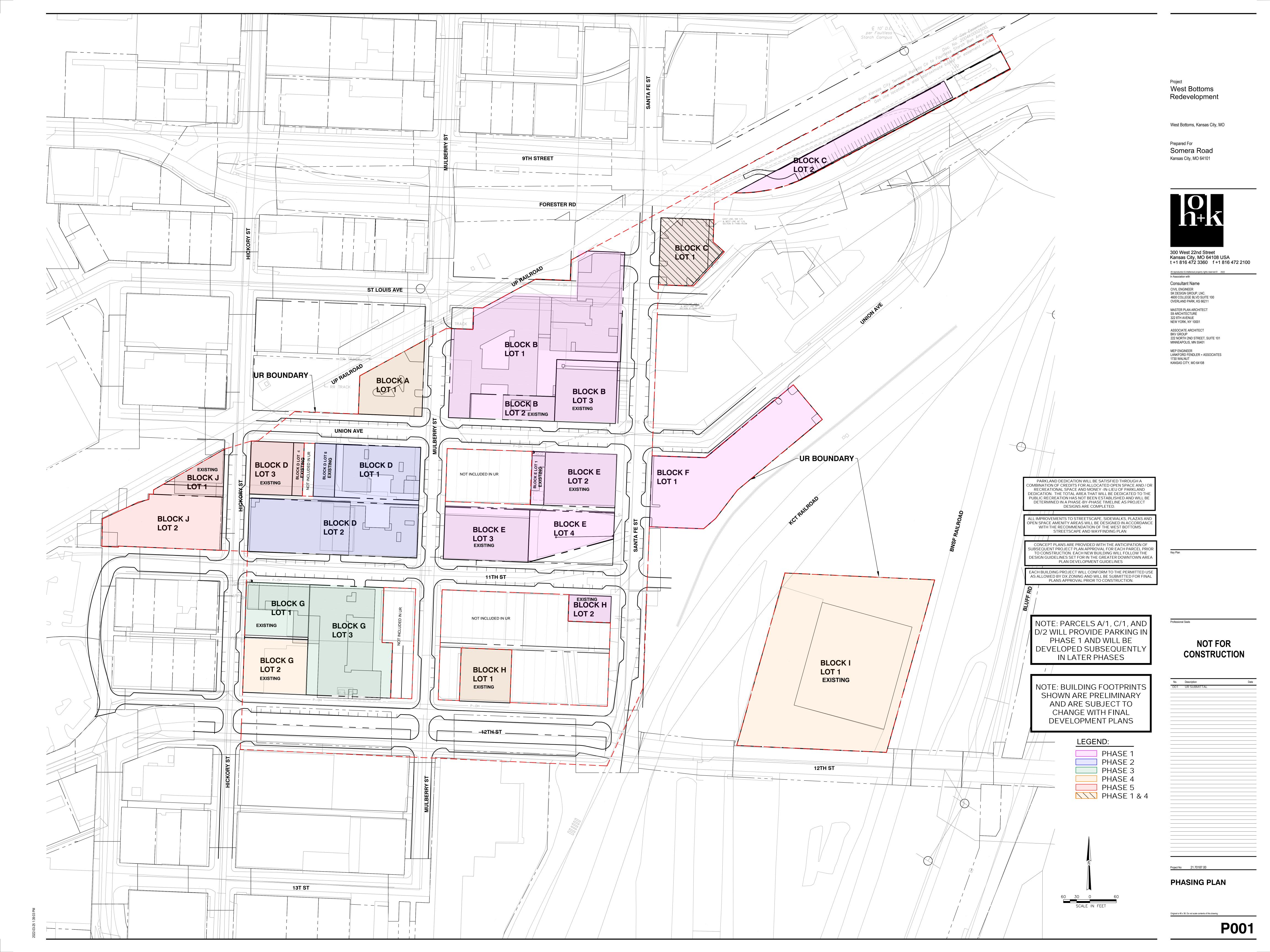
A TRACT OF LAND BEING PART OF THE NORTHEAST QUARTER, THE SOUTHWEST QUARTER OF SECTION 6, TOWNSHIP 49 NORTH, RANGE 33 WEST, TURNER AND CO'S ADDITION, PART OF BLOCKS 39, 40, 42, 43, 44, 45, 53, 54, 57 AND 58, WEST KANSAS ADDITION PART OF BLOCKS 28 AND 29. ALL IN THE CITY OF KANSAS CITY, JACKSON COUNTY, MISSOURI DESCRIBED AS:

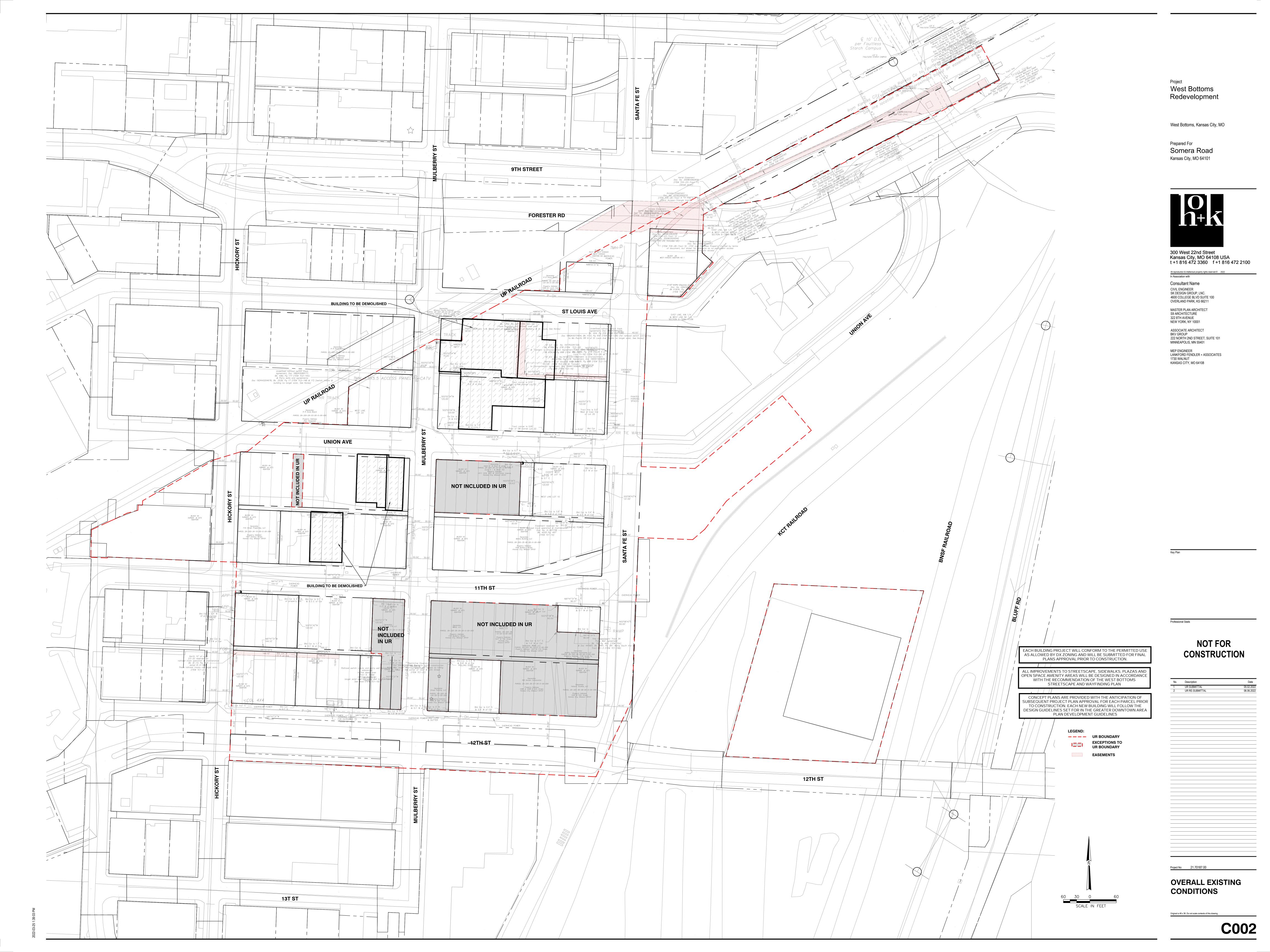
BEGINNING AT THE NORTHWEST CORNER OF LOT 16, OF BLOCK 3 OF WEST KANSAS 2ND ADDITION IN SAID SOUTHWEST CORNER OF BLOCK 3 OF WEST KANSAS 2ND ADDITION IN SAID SOUTHWEST CORNER OF BLOCK 58 OF SAID TURNER AND CO'S ADDITION; THENCE NORTH 02°01'25" EAST 414.37 FEET TO THE SOUTHWEST CORNER OF LOT 32 OF BLOCK 44 OF SAID TURNER AND CO'S ADDITION; THENCE NORTH 87°40'37" WEST ALONG THE SOUTH LINE OF SAID NORTHWEST QUARTER; THENCE SOUTH 01°56'54" WEST ALONG THE EAST LINE OF THE SOUTHEAST QUARTER OF SAID SOUTH WEST 38.24 FEET TO A POINT 2.01 FEET; THENCE NORTH 64°55'20" EAST 61.92 FEET; THENCE NORTH 64°55'20" EAST 43.05 FEET TO A POINT ON THE EAST LINE OF THE SOUTHEAST QUARTER OF SAID BLOCK 45; THENCE NORTH 65°18'55" EAST 19.42 FEET TO A POINT ON THE WEST LINE OF LOT 25 OF SAID BLOCK 45; THENCE NORTH 63° 05'10" EAST 27.43 FEET TO A POINT ON THE EAST LINE OF LOT 25 OF SAID BLOCK 45, 18.69 FEET SOUTH OF THE NORTHEAST CORNER OF LOT 25 OF SAID BLOCK 45, 11.03 FEET SOUTH OF THE NORTHEAST CORNER OF LOT 24 OF SAID BLOCK 45; THENCE NORTH 02°02'41" EAST 11.03 FEET TO THE NORTH EAST 10.00 FEET TO A POINT ON THE SOUTH LINE OF LOT 10 OF SAID BLOCK 45: THENCE NORTH 60°08'56" EAST ALONG THE SOUTHERLY LINE OF LOT 16 OF SAID BLOCK 45. SAID POINT BEING 21.59 FEET SOUTH OF THE NORTHEAST CORNER OF LOT 16 OF SAID BLOCK 45. 45; THENCE NORTH 86°40'53" EAST 60.17 FEET TO A POINT ON THE WEST LINE OF LOT 1 OF SAID BLOCK 44; THENCE NORTH 51°13'28" EAST 106.53 FEET TO A POINT ON THE SOUTH LINE OF BLOCK 39 OF SAID TURNER AND CO'S ADDITION; THENCE SOUTH 87°41'45" EAST 144.31 FEET TO THE SOUTHEAST CORNER OF LOT 23 86.70 FEET TO A POINT ON THE SOUTHERLY RIGHT OF WAY OF THE UNION PACIFIC RAILROAD; THENCE NORTH 61°16'47" EAST ALONG THE SOUTHERLY RIGHT OF WAY OF SAID UNION PACIFIC RAILROAD. 223.37 FEET: THENCE SOUTH 87°44'54" EAST 60.49 FEET TO A POINT ON THE WEST LINE OF LOT 3 OF BLOCK 40 OF SAID TURNER AND CO'S ADDITION; THENCE NORTH 02°07'16" EAST 50.00 FEET TO THE NORTHWEST CORNER OF SAID LOT 3; THENCE SOUTH 87°44'54" EAST ALONG THE NORTH LINE OF BLOCK 29 OF SAID WEST KANSAS ADDITION; THENCE SOUTH 87°44'54" EAST 78.65 FEET TO THE SOUTHEAST CORNER OF LOT 23 OF SAID BLOCK 29; THENCE NORTH 02°14'35" EAST ALONG THE EAST LINE OF SAID UNION PACIFIC RAILROAD; THENCE NORTH 61°36'35" EAST ALONG THE SOUTHERLY RIGHT OF WAY LINE OF SAID UNION PACIFIC RAILROAD 371.36 FEET TO A POINT ON THE WEST LINE OF THE NORTHEAST QUARTER OF SAID UNION PACIFIC RAILROAD, 711.10 FEET; THENCE SOUTH 28°01'41" EAST 88.22 FEET TO A POINT ON THE NORTH RIGHT OF WAY LINE OF BRUCE FORESTER VIADUCT; THENCE SOUTH 61°56'44" WEST ALONG THE NORTH RIGHT OF WAY LINE OF SAID BRUCE FORESTER VIADUCT, 0.39 FEET; THENCE SOUTH 62°11'14" WEST ALONG THE NORTH LINE OF SAID BRUCE FORESTER VIADUCT, 231.79 FEET; THENCE WESTERLY ALONG A CURVE TO THE RIGHT HAVING A RADIUS OF 340.00 FEET AND AN ARC LENGTH OF 175.51 FEET; THENCE NORTH 87°49'35" WEST 3.28 FEET; THENCE SOUTH 60°32'12" WEST 48.72 FEET TO A POINT ON THE WEST LINE OF SAID NORTHEAST QUARTER; THENCE SOUTH 48°21'39" EAST 33.26 FEET TO A POINT ON THE NORTH RIGHT OF WAY OF ST. LOUIS AVE. AS NOW ESTABLISHED; THENCE SOUTH 42°11'21" WEST ALONG THE NORTH RIGHT OF WAY LINE OF SAID ST. LOUIS AVE., 78.00 FEET TO A POINT ON THE EAST LINE OF SANTA FE STREET AS NOW ESTABLISHED; THENCE SOUTH 02°14'17" WEST ALONG THE EAST LINE OF SAID SANTA FE STREET 387.93 FEET TO THE NORTHEAST CORNER OF LOT 5 OF SAID BLOCK 42, SAID POINT BEING ON THE WEST LINE OF THE NORTHEAST QUARTER OF SAID SECTION 6; THENCE SOUTH 50°11'21" EAST ALONG THE SOUTH 89°53'30" EAST 102.55 FEET; THENCE SOUTH 50°11'21" WEST PARALLEL WITH THE SOUTH RIGHT OF WAY LINE OF SAID UNION AVE., 250.65 FEET: THENCE SOUTH 37°32'32" WEST 80,98 FEET: THENCE SOUTH 49°27'54" WEST 35,74 FEET TO A POINT ON THE WEST LINE OF THE NORTHEAST CORNER OF LOT 6 OF BLOCK 42 OF SAID TURNER AND CO'S ADDITION: THENCE NORTH 87°53'02" WEST 123.19 FEET TO THE SOUTHWEST CORNER OF SAID LOT 6, SAID POINT BEING ON THE EAST RIGHT OF WAY LINE OF SAID SANTA FE STREET; THENCE SOUTH 24°50'22" WEST 194.76 FEET TO A

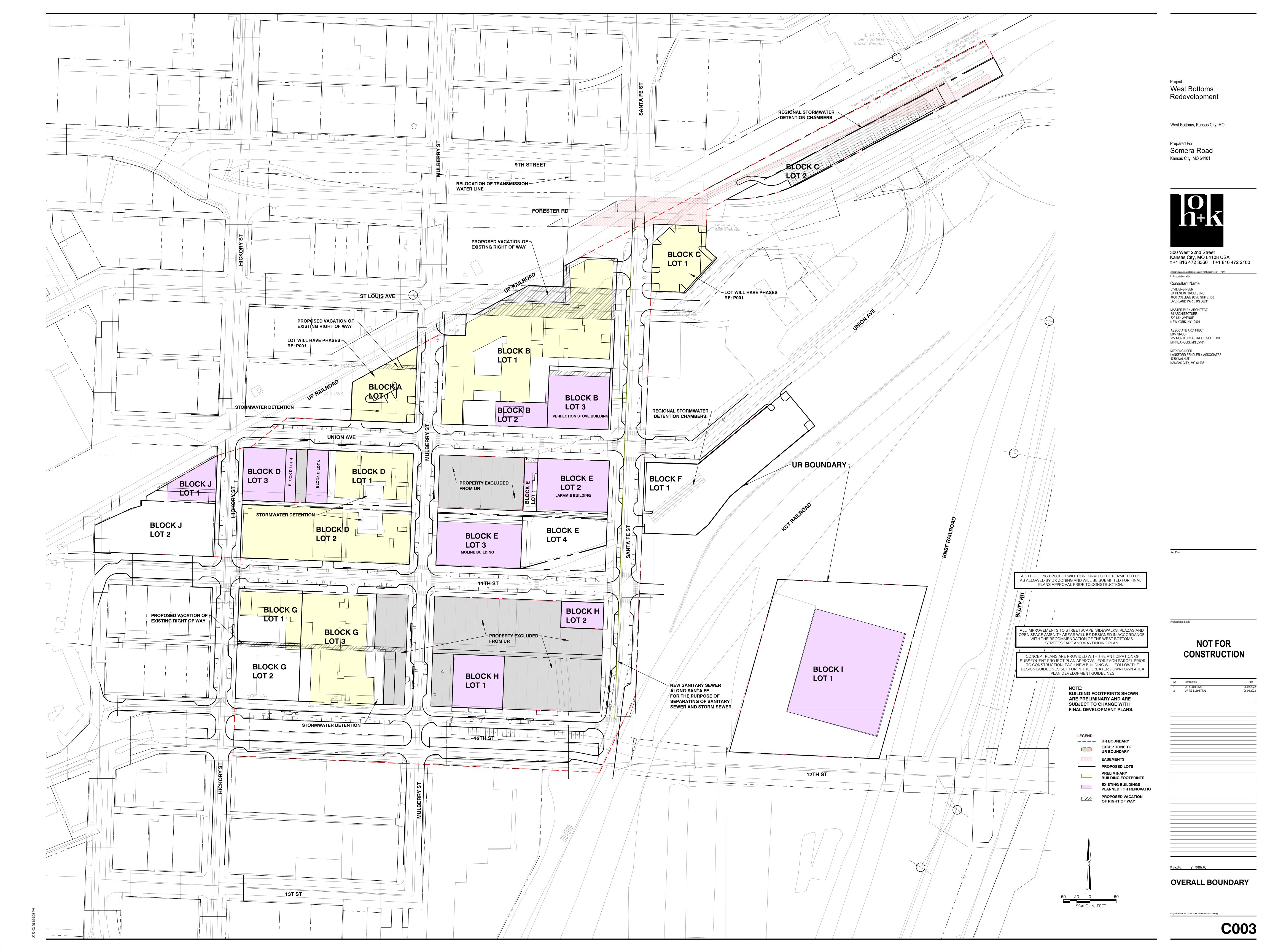
NORTH 87°25'28" WEST 383.69 FEET TO THE POINT OF BEGINNING. A TRACT OF LAND BEING PART OF THE NORTHEAST QUARTER OF SECTION 6, TOWNSHIP 49 NORTH, TOWNSHIP 33 WEST, IN THE CITY OF KANSAS CITY, JACKSON COUNTY, MISSOURI DESCRIBED AS: COMMENCING AT THE SOUTHWEST CORNER OF THE NORTHEAST QUARTER OF SAID NORTHEAST QUARTER, 91.21 FEET; THENCE NORTH 02°38'49" EAST 30.00 FEET TO A POINT ON THE NORTH RIGHT OF WAY LINE OF WEST 12TH STREET AND THE POINT OF BEGINNING; THENCE NORTH 15°37'19" EAST 406.35 FEET; THENCE SOUTH 87°21'11" WEST 339.92 FEET; THENCE SOUTH 87°21'11" WEST 339.92 FEET; THENCE SOUTH 15°37'19" WEST 406.35 FEET TO A POINT ON THE NORTH RIGHT OF WAY LINE OF SAID WEST 12TH STREET; THENCE NORTH 87°21'11" WEST 339.92 FEET TO THE POINT

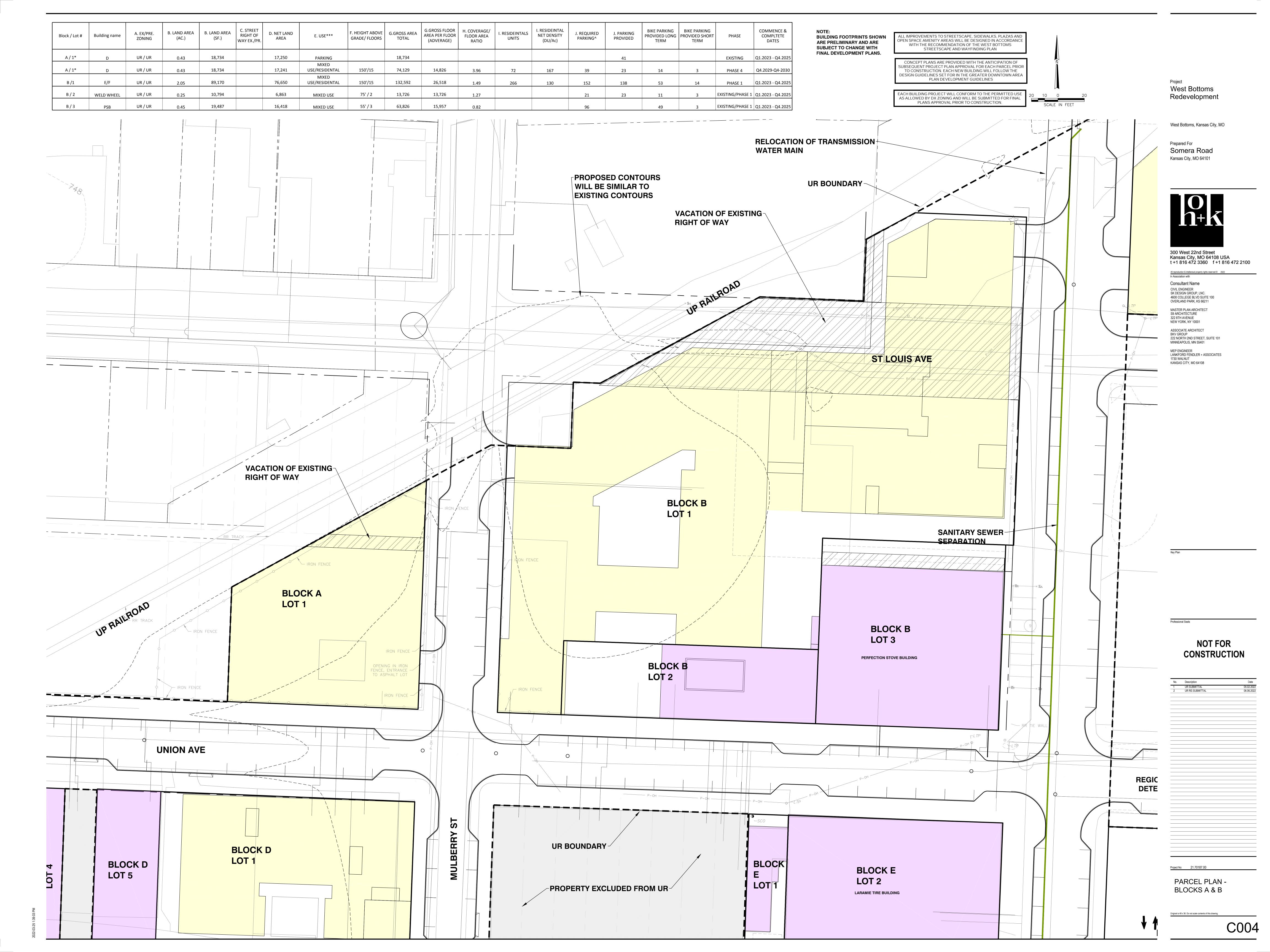
POINT ON THE SOUTH RIGHT OF WAY LINE OF 12TH STREET IN THE SOUTHWEST QUARTER OF SAID 12TH STREET, 448.86 FEET TO THE NORTHEAST CORNER OF LOT 1, BLOCK 3 WEST KANSAS 2ND ADDITION; THENCE

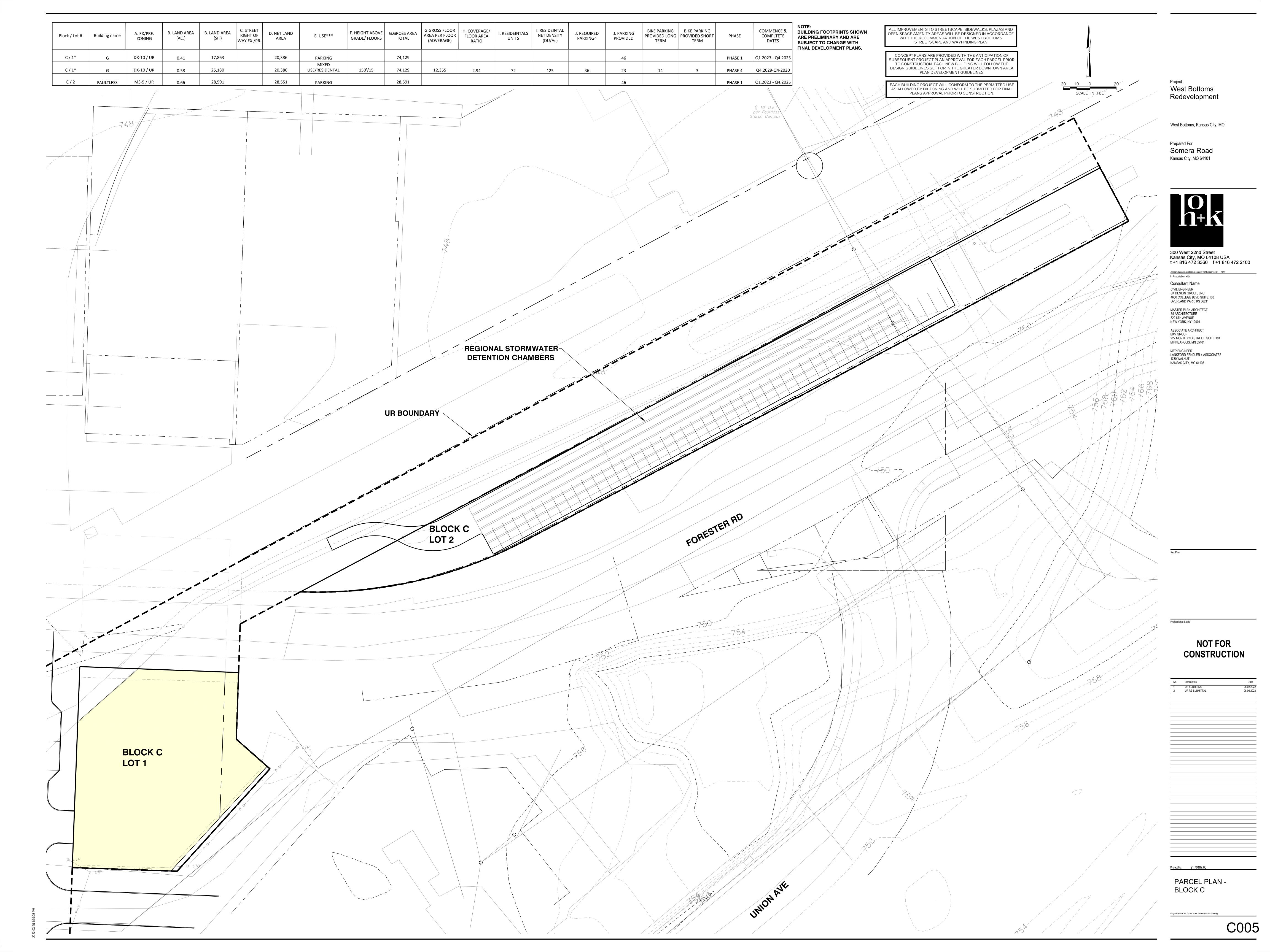
EXCEPT 21, 22, 23, 24, 25, THE EAST 4.95 OF LOT 26, LOTS 31, 32, AND THE 10.0' ALLEY IN BLOCK 54, IN TURNER AND CO'S ADDITION TO SAID CITY OF KANSAS CITY.



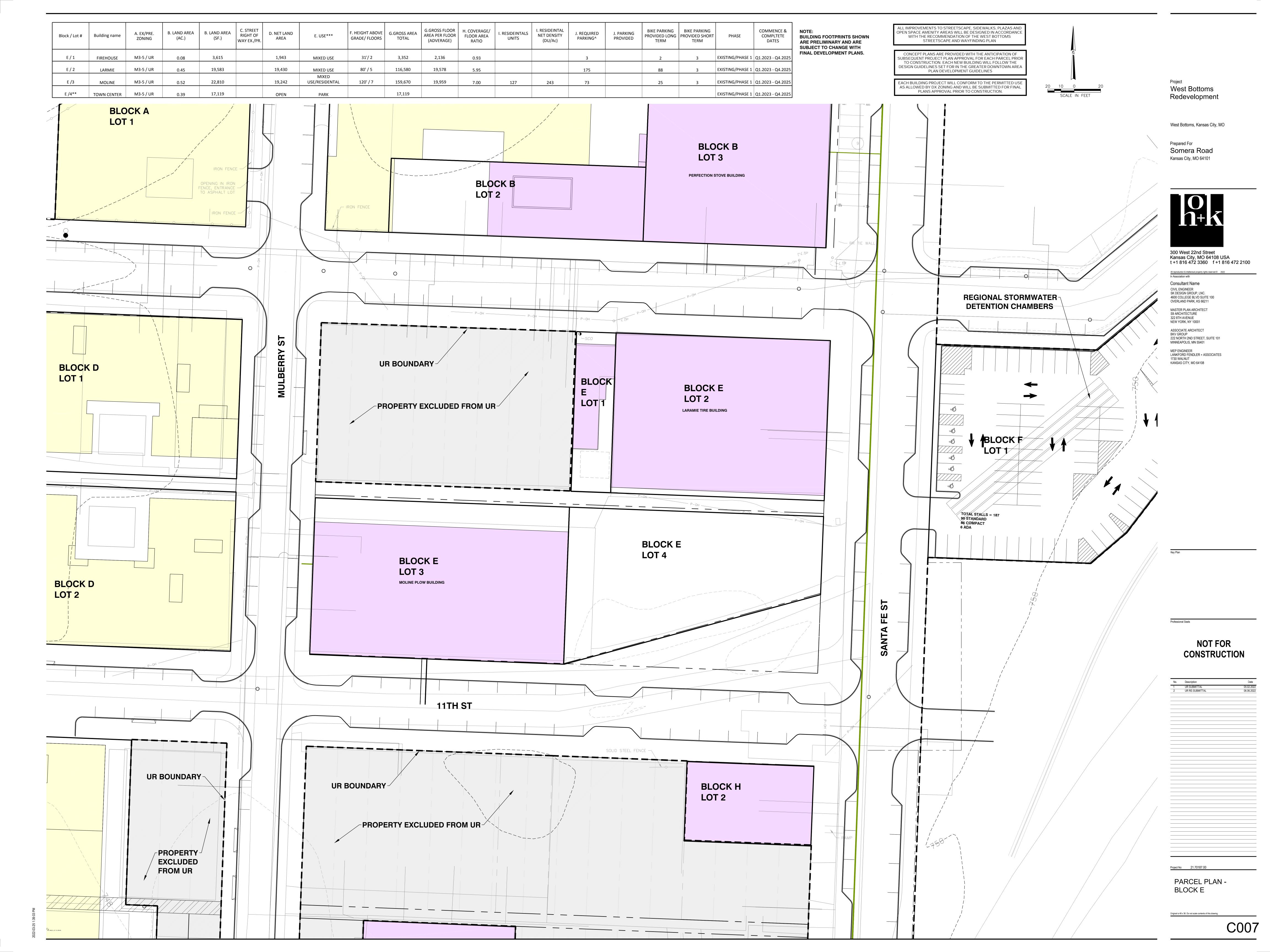


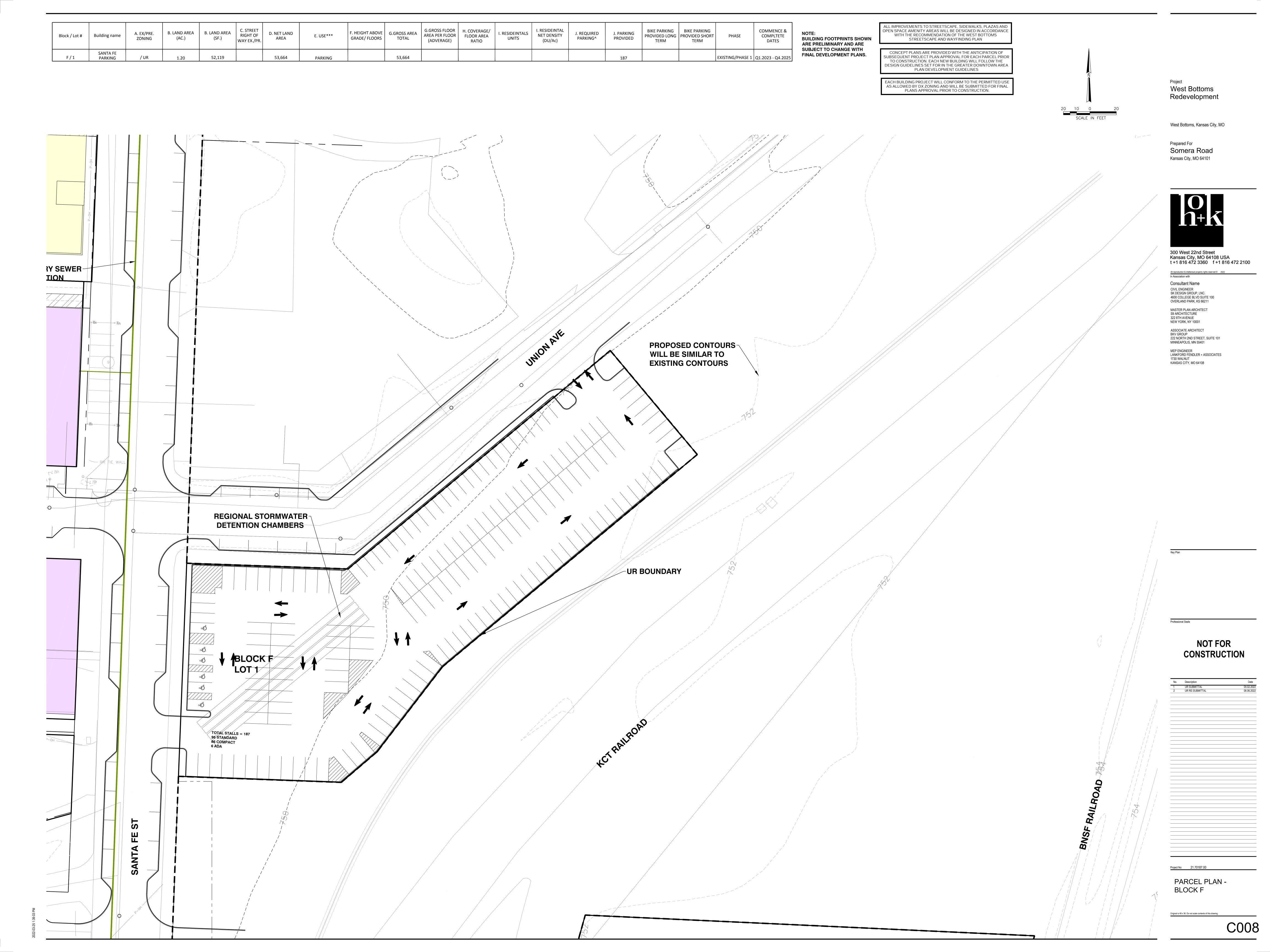




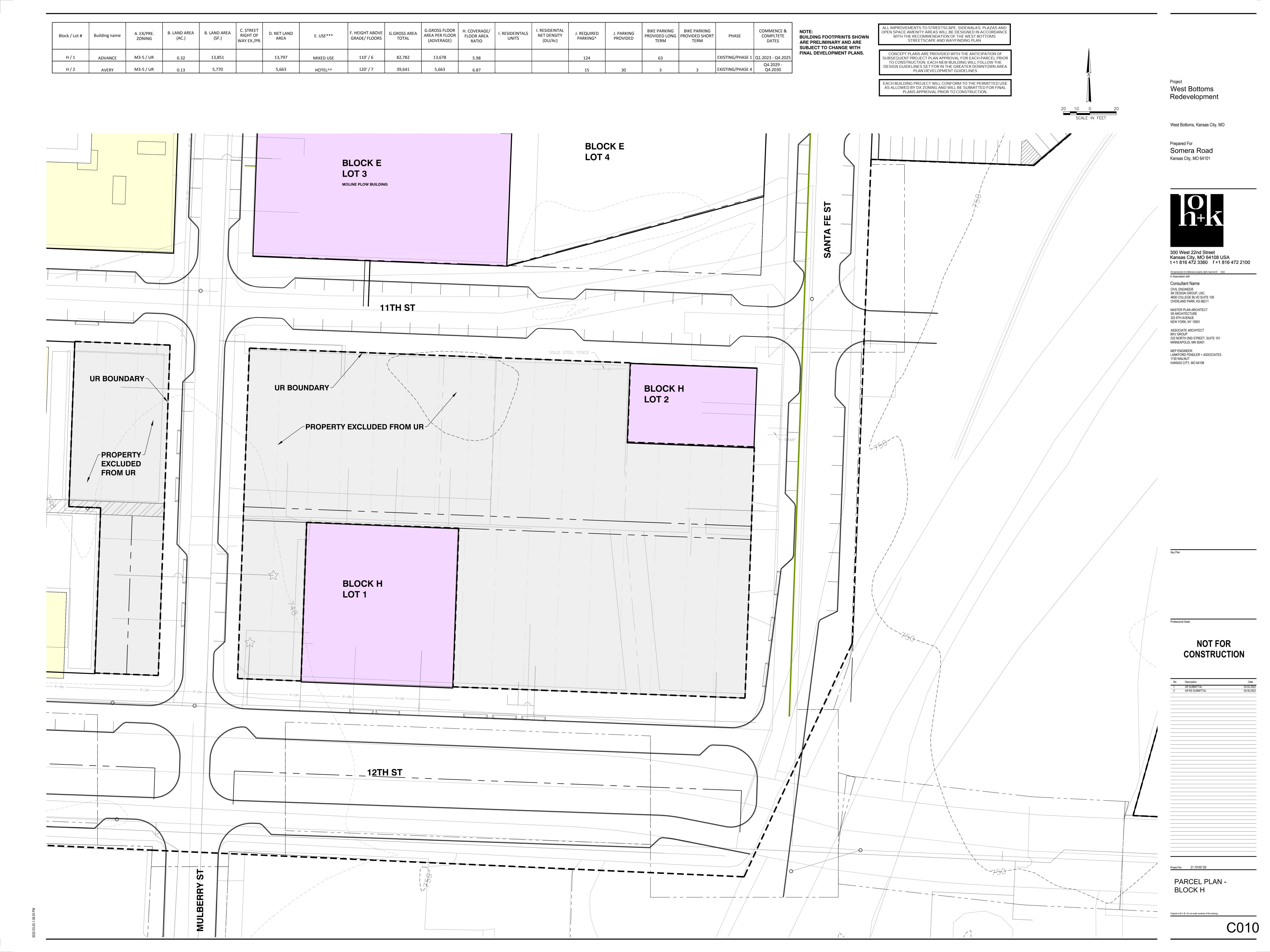


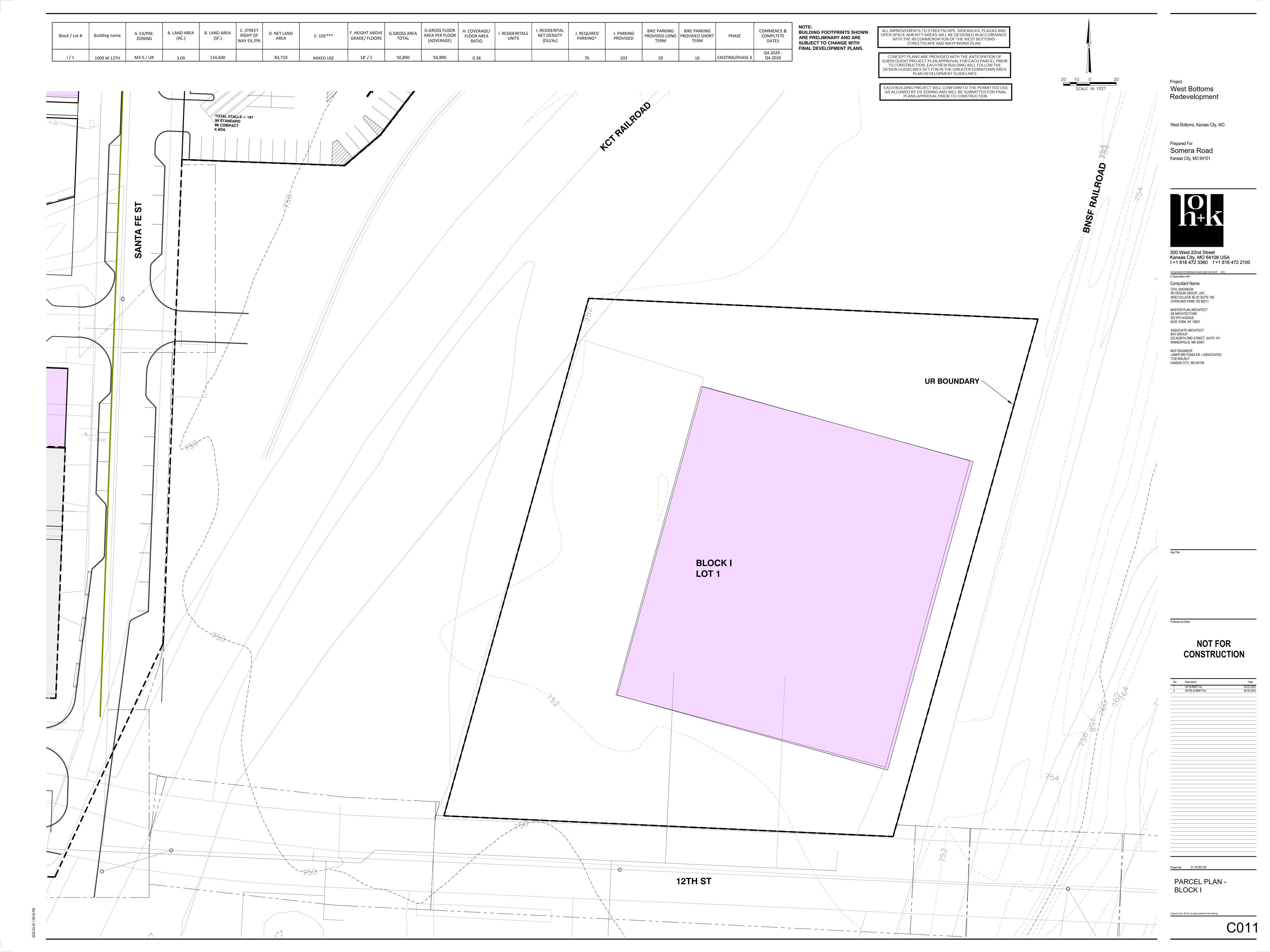
D/3 NATHAN M3-5/UR 0.26 11,473 11,552 USE/RESIDENTAL 55/4 46,208 11,552 4.03 43 164 33 9 3 EXISTING/PHASE 5 Q1.2030 - Q4.2035 D/4 1323 M3-5/UR 0.07 2,897 2,888 USE/RESIDENTAL 45/3 8,664 2,888 2.99 11 163 8 2 3 EXISTING/PHASE 5 Q1.2030 - Q4.2035 D/5 1317 M3-5/UR 0.13 5,808 4,835 USE/RESIDENTAL 31/2 9,670 4,835 1.66 12 91 9 2 3 EXISTING/PHASE 2 Q4.2024-Q4.2026	Project West Bottoms Redevelopment West Bottoms, Kansas City, MO Prepared For
BLOCK A LOT 1 BLOCK B LOT 2	Somera Road Kansas City, MO 64101 300 West 22nd Street Kansas City, MO 64108 USA t +1 816 472 3360 f +1 816 472 2100 All regended on & Individual properly rights reserved © 3022 in Association with Consultant Name CIVIL ENGINEER SKD ESIGN GROUP, LNC. 4600 COLLEGE BLVD SUITE 100 OVERLAND PARK KS 66211 MASTER PLAN ARCHITECT SO ARCHITECTURE 322 8TH AVENUE NEW YORK, NY 10001 ASSOCIATE ARCHITECT BKV GROUP 222 NORTH ZND STREET, SUITE 101 MINNEAPOLIS, MN 55401 MEP ENGINEER LANKFORD FEADLER + ASSOCIATES 1730 WALNUT KANSAS CITY, MO 64108
BLOCK J LOT 2 PROPOSE CONTURS MIL BE SMILAR TO EXETING CONTURS LOT 2 MARKET MIL BE SMILAR TO EXETING CONTURS UT STREET 11TH ST	Project No: 21.70187.00 PARCEL PLAN - BLOCK D Cognal is 48x 36. Do not scale contents of this drawing.

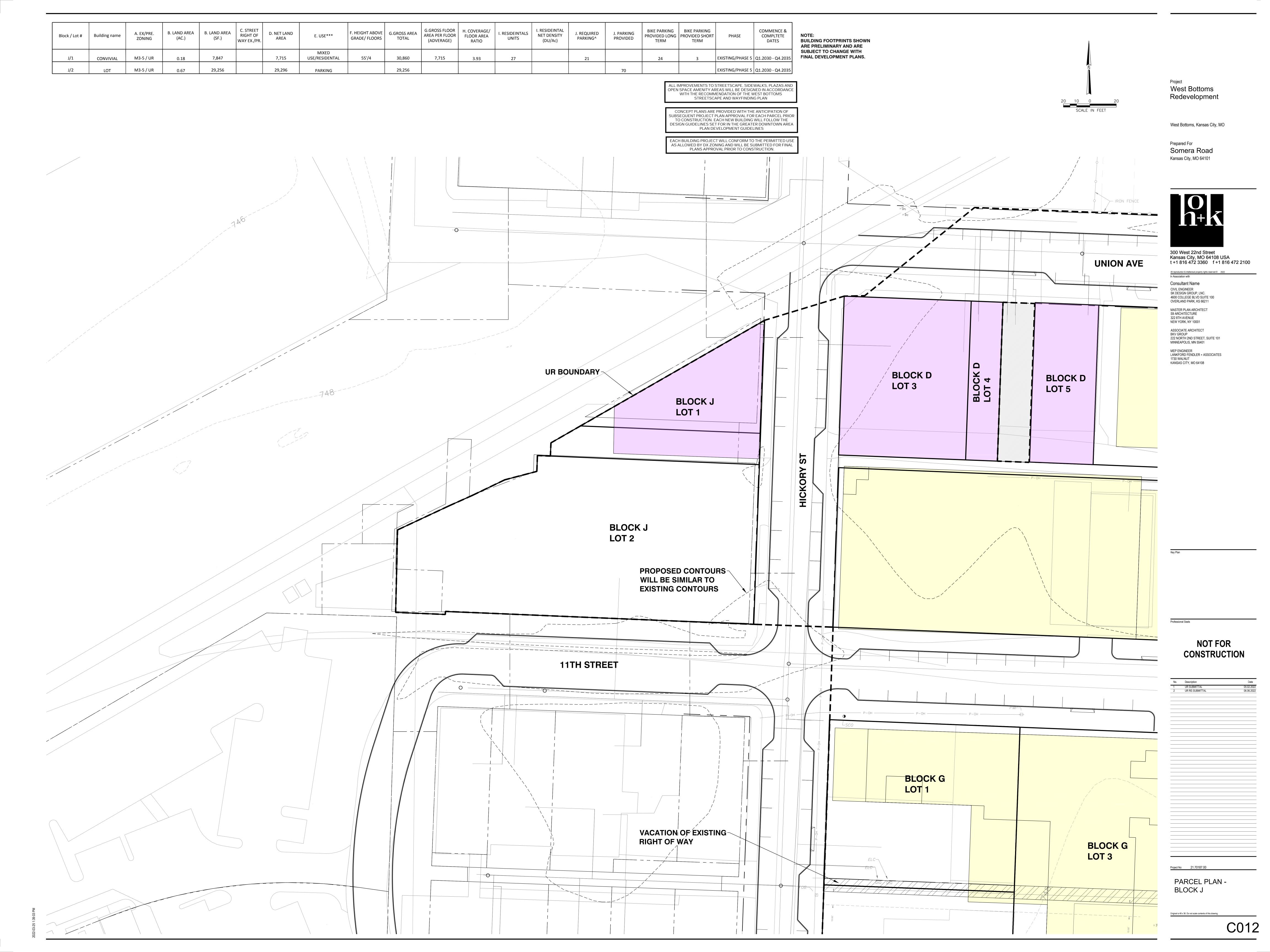


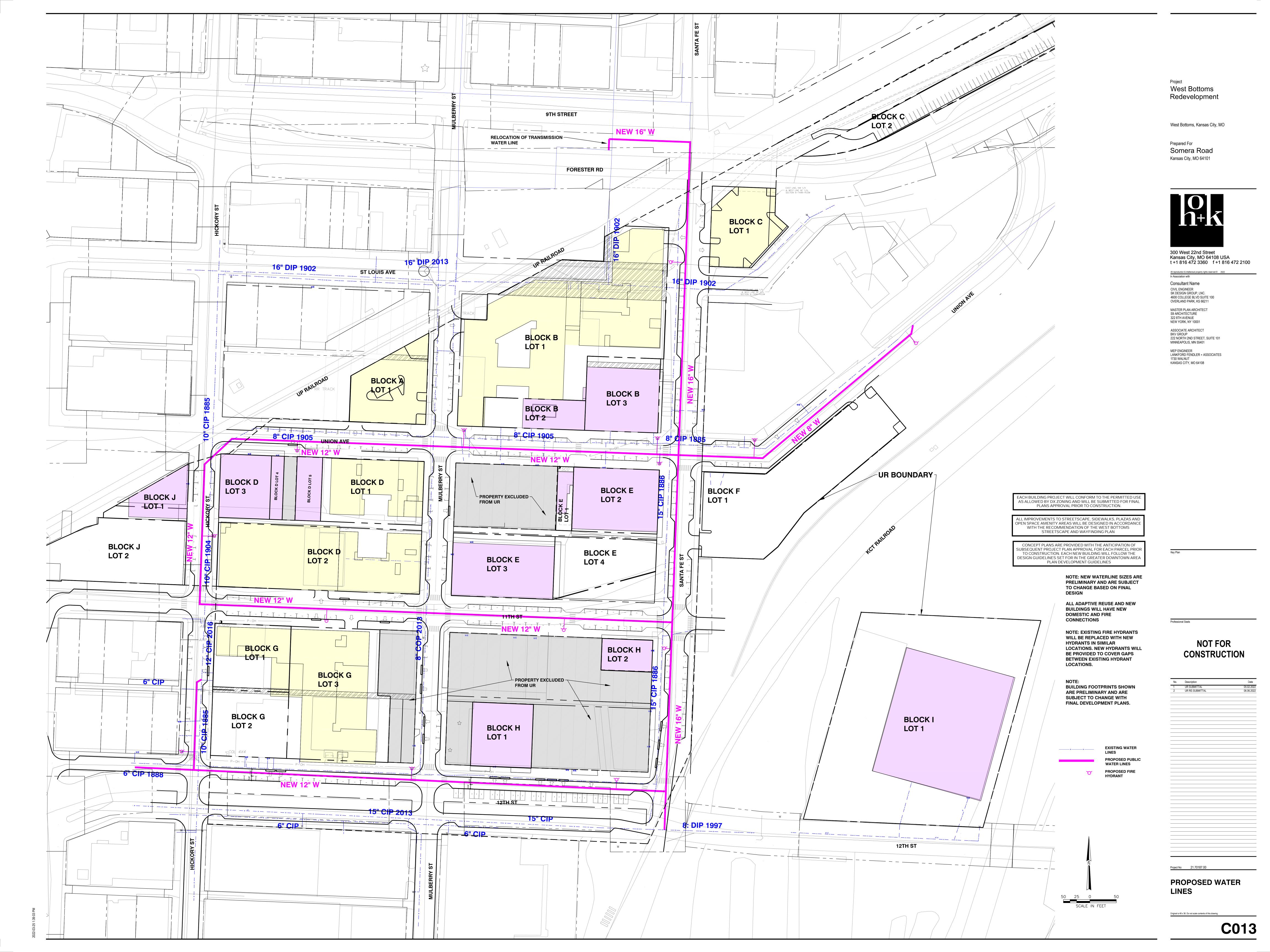


Block / Lot # Building name A. EX/PRE. ZONING B. LAND AREA (AC.) B. LAND AREA (SF.) D. NET LAND AREA (SF.) D. NET LAND AREA (AC.) D. NET LAND AREA (SF.) D. NET LAND AREA (AC.) D. NET LAND AREA (SF.) D. NET LAND AREA (BRADE/FLOORS (ADVERAGE) (AC.) D. NET LAND AREA (BRADE/FLOORS (ADVERAGE) (ADVER	1.96 31 75 21 22 6 3 EXISTING/PHASE 3 Q4.2026 Q4.2028 2.84 45 108 33 14 9 3 EXISTING/PHASE 4 Q4.2026 Q4.2028 5.74 236 228 126 184 47 18 PHASE 3 Q4.2028 EACH BUILDING PROJECT WILL CONFORM TO THE PERMITTED USE AS ALLOWED BY DX ZONING AND WILL BE SUBMITTED FOR FINAL PLANS APPROVAL PRIOR TO CONSTRUCTION. EACH BUILDING PROJECT WILL CONFORM TO THE PERMITTED USE AS ALLOWED BY DX ZONING AND WILL BE SUBMITTED FOR FINAL PLANS APPROVAL PRIOR TO CONSTRUCTION. EACH BUILDING PROJECT WILL CONFORM TO THE PERMITTED USE AS ALLOWED BY DX ZONING AND WILL BE SUBMITTED FOR FINAL PLANS APPROVAL PRIOR TO CONSTRUCTION. EACH BUILDING PROJECT WILL CONFORM TO THE PERMITTED USE AS ALLOWED BY DX ZONING AND WILL BE SUBMITTED FOR FINAL PLANS APPROVAL PRIOR TO CONSTRUCTION. EACH BUILDING PROJECT WILL CONFORM TO THE PERMITTED USE AS ALLOWED BY DX ZONING AND WILL BE SUBMITTED FOR FINAL PLANS APPROVAL PRIOR TO CONSTRUCTION. EACH BUILDING PROJECT WILL CONFORM TO THE PERMITTED USE AS ALLOWED BY DX ZONING AND WILL BE SUBMITTED FOR FINAL PLANS APPROVAL PRIOR TO CONSTRUCTION. EACH BUILDING PROJECT WILL CONFORM TO THE PERMITTED USE AS ALLOWED BY DX ZONING AND WILL BE SUBMITTED FOR FINAL PLANS APPROVAL PRIOR TO CONSTRUCTION.	Project West Bottoms Redevelopment West Bottoms, Kansas City, MO
PROPOSED CONTOURS WILL BE SIMILAR TO EXISTING CONTOURS	BLOCK E LOT 3 MOLINE PLOW BUILDING LOT 2	Prepared For Somera Road Kansas City, MO 64101
11TH STREET	11TH ST P-04 P-04 P-04 P-05 P-0	ASSOCIATE ARCHITECT BY ORCH PLAN ARCHITECT BY ARCHITECTURE BY ORK, NY 10001 ASSOCIATE ARCHITECT BY ORCH PLAN BY 10001 AS
VACATION OF EXISTING RIGHT OF WAY	PROPERTY EXCLUDED FROM UR BLOCK G LOT 3 PROPERTY EXCLUDED FROM UR	CANSAS CITY, MO 64108
UR BOUNDARY BLOCK G LOT 2		Gey Plan
FCOL 4X4	P-OH P-OH P-OH P-OH P-OH	NO. Description Date 1 UR SUBMITTAL 05.02.2 2 UR RE-SUBMITTAL 06.06.2
	12TH ST	
UR BOUNDARY DE LA COMPANIA DEL COMPANIA DE LA COMPANIA DEL COMPANIA DE LA COMPANIA DEL COMPANIA DE LA COMPANIA DEL COMPANIA DE LA COMPANIA DEL COMPANIA DE LA COMPANIA DEL COMPANIA DE LA COMPANIA DE LA COMPANIA DE LA COMPANIA DE LA	ULBERRY ST	roject No: 21.70187.00 PARCEL PLAN - BLOCK G riginal is 48 x 36. Do not scale contents of this drawing.









SomeraRoad

West Bottoms Redevelopmer

West Bottoms, Kansas City, MO

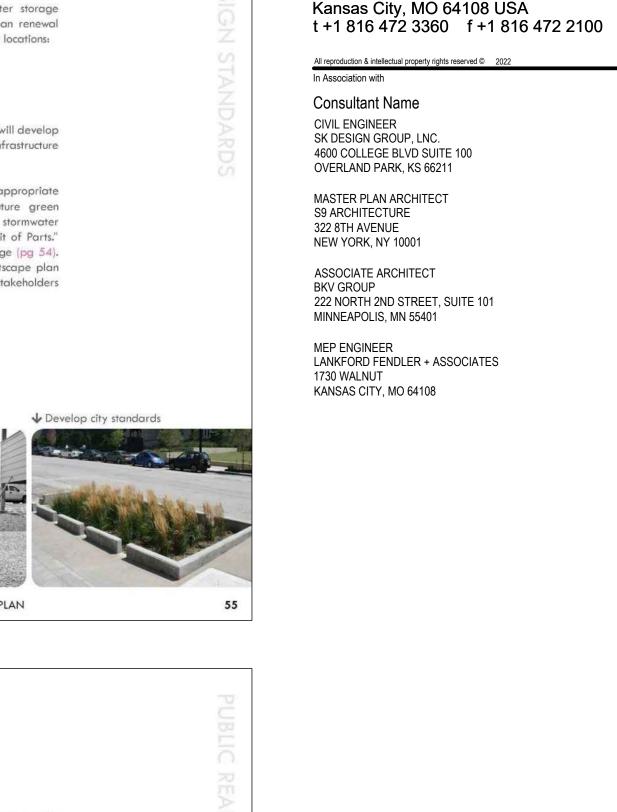
Prepared For Somera Road Kansas City, MO 64101

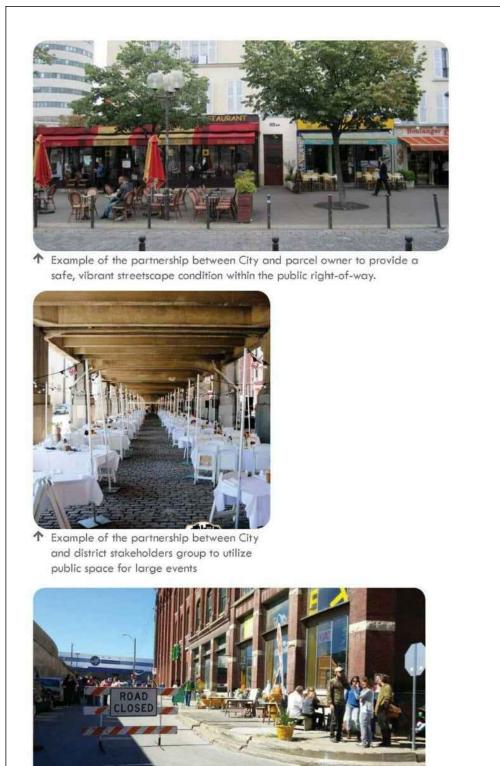
300 West 22nd Street



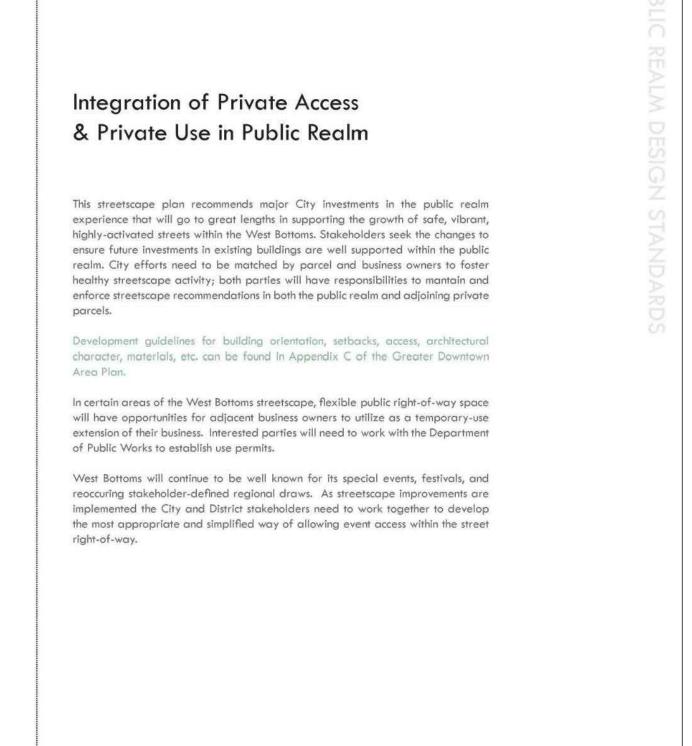






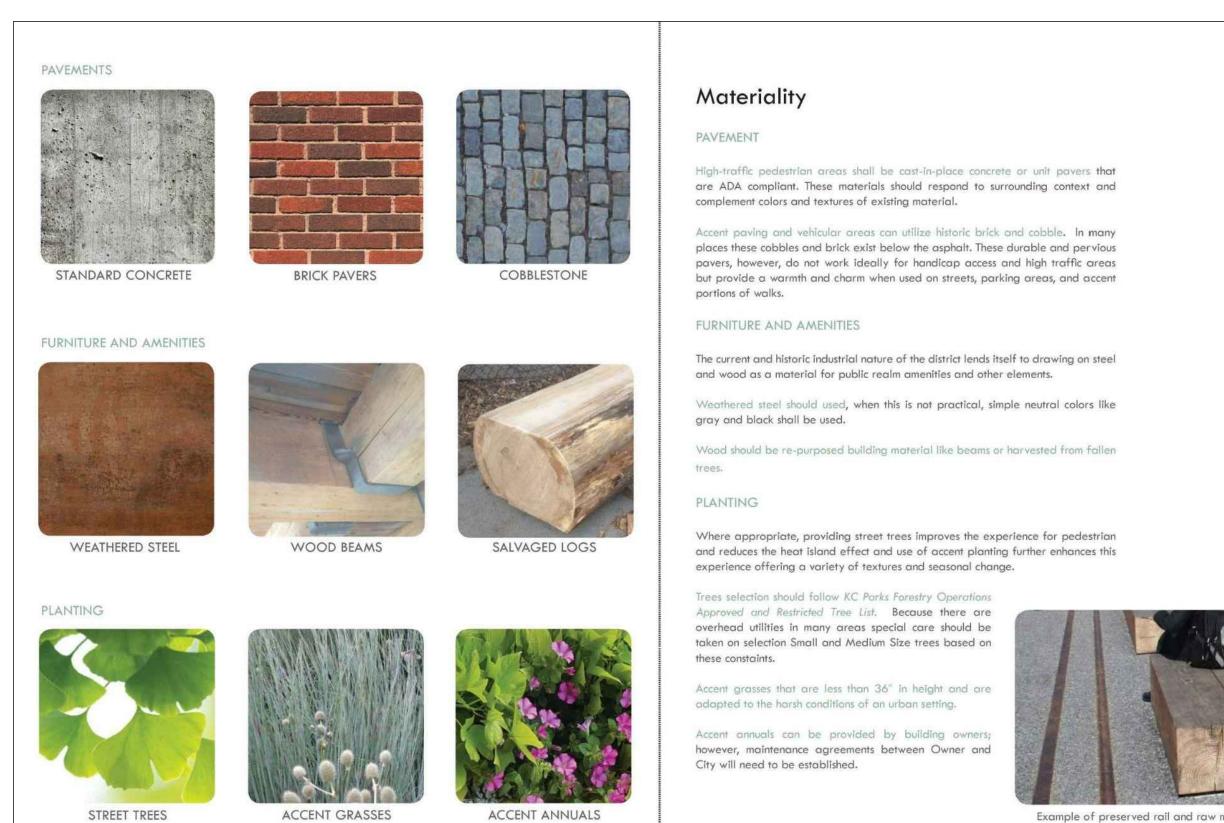


↑ Future streetscape improvements will provide more permanent amenities

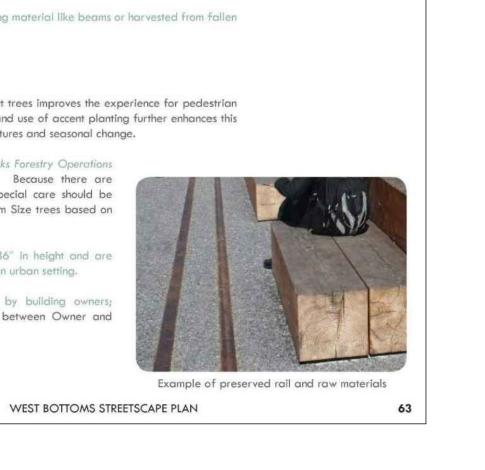


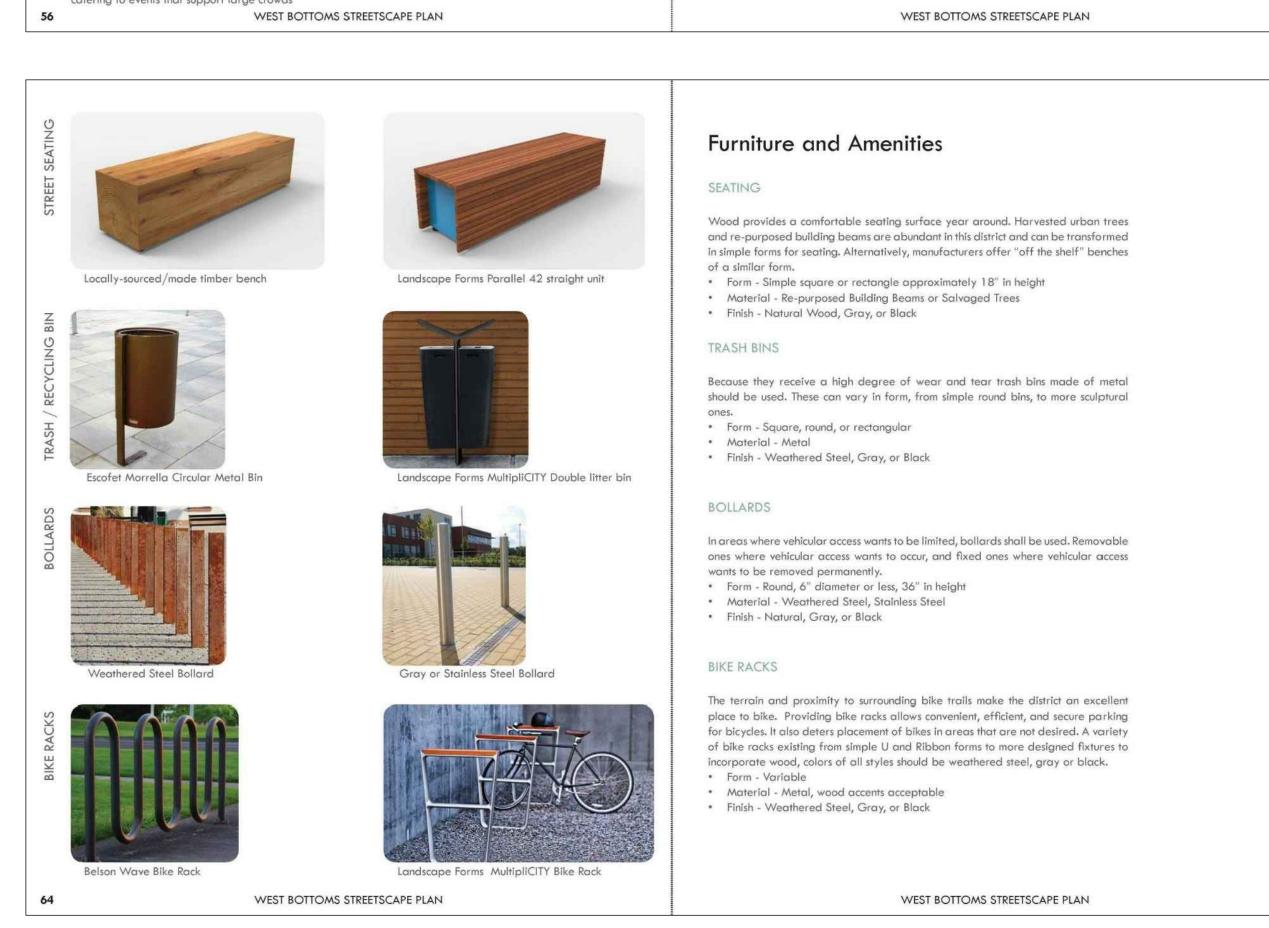


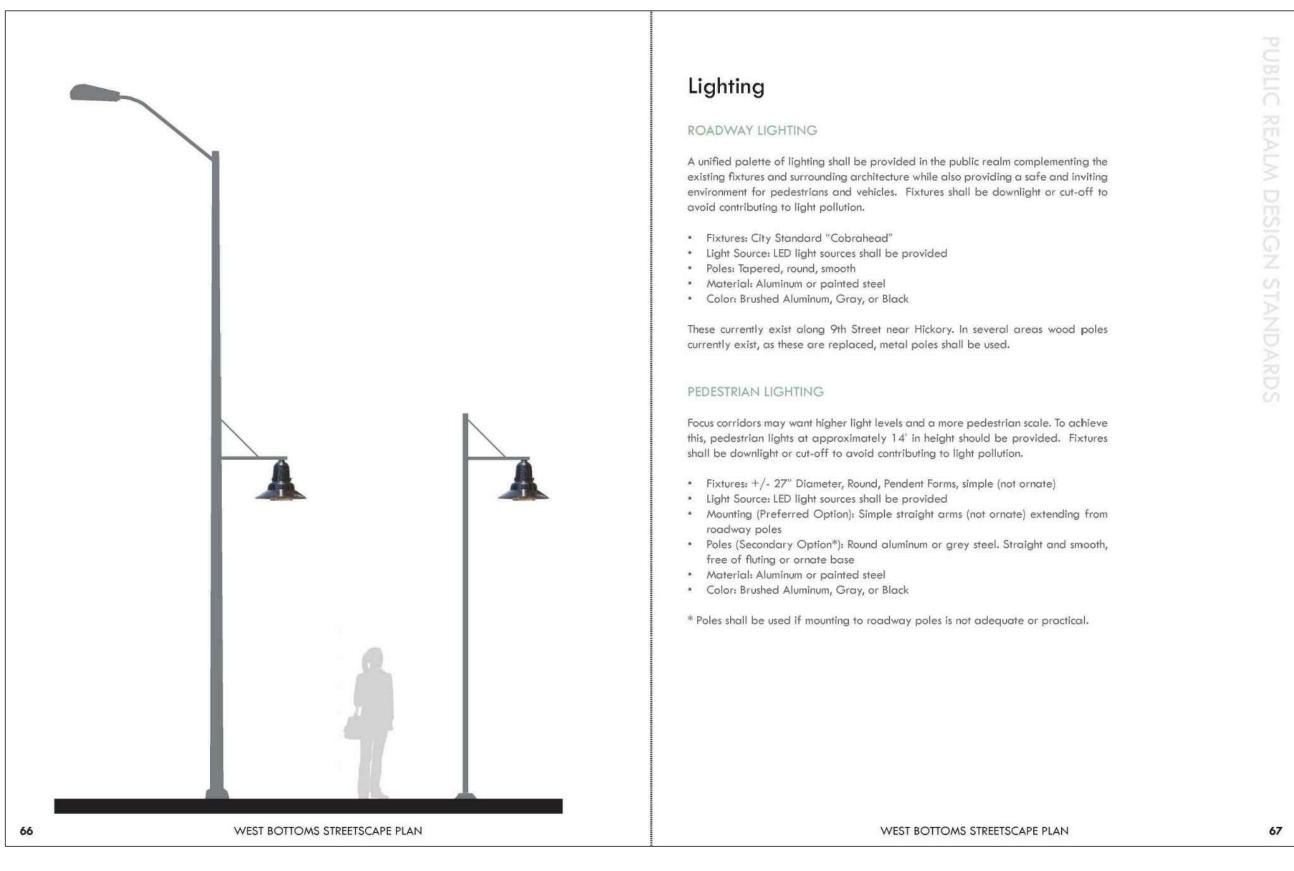




WEST BOTTOMS STREETSCAPE PLAN





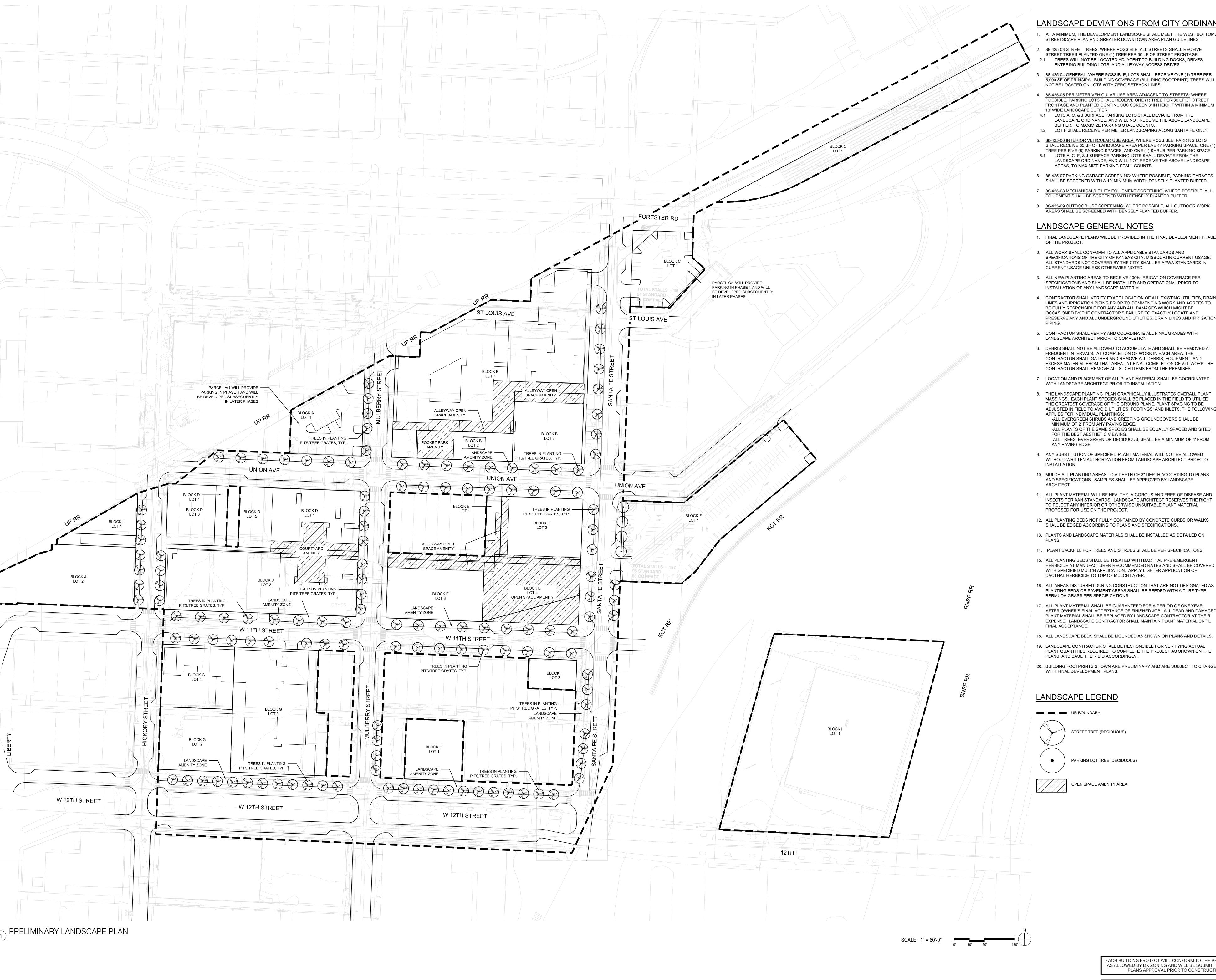


EACH BUILDING PROJECT WILL CONFORM TO THE PERMITTED USE AS ALLOWED BY DX ZONING AND WILL BE SUBMITTED FOR FINAL PLANS APPROVAL PRIOR TO CONSTRUCTION. ALL IMPROVEMENTS TO STREETSCAPE, SIDEWALKS, PLAZAS AND OPEN SPACE AMENITY AREAS WILL BE DESIGNED IN ACCORDANCE WITH THE RECOMMENDATION OF THE WEST BOTTOMS STREETSCAPE AND WAYFINDING PLAN CONCEPT PLANS ARE PROVIDED WITH THE ANTICIPATION OF SUBSEQUENT PROJECT PLAN APPROVAL FOR EACH PARCEL PRIOR TO CONSTRUCTION. EACH NEW BUILDING WILL FOLLOW THE DESIGN GUIDELINES SET FOR IN THE GREATER DOWNTOWN AREA PLAN DEVELOPMENT GUIDELINES GENERAL NOTES: 1. CONCEPT LANDSCAPING PLANS ARE PROVIDED IN UR SUBMITTAL WITH THE ANTICIPATION OF SUBSEQUENT PROJECT PLAN APPROVAL FOR EACH PARCEL PRIOR TO CONSTRUCTION. EACH NEW BUILDING WILL FOLLOW THE DESIGN

GUIDELINES SET FORTH IN THE GREATER DOWNTOWN AREA PLAN

DEVELOPMENT STANDARDS.

STREETSCAPE DEVELOPMEN^{*} **GUIDELINES**



LANDSCAPE DEVIATIONS FROM CITY ORDINANCE 88-425

1. AT A MINIMUM, THE DEVELOPMENT LANDSCAPE SHALL MEET THE WEST BOTTOMS STREETSCAPE PLAN AND GREATER DOWNTOWN AREA PLAN GUIDELINES.

88-425-03 STREET TREES: WHERE POSSIBLE, ALL STREETS SHALL RECEIVE STREET TREES PLANTED ONE (1) TREE PER 30 LF OF STREET FRONTAGE. 2.1. TREES WILL NOT BE LOCATED ADJACENT TO BUILDING DOCKS, DRIVES ENTERING BUILDING LOTS, AND ALLEYWAY ACCESS DRIVES.

88-425-04 GENERAL: WHERE POSSIBLE, LOTS SHALL RECEIVE ONE (1) TREE PER 5,000 SF OF PRINCIPAL BUILDING COVERAGE (BUILDING FOOTPRINT). TREES WILL NOT BE LOCATED ON LOTS WITH ZERO SETBACK LINES.

88-425-05 PERIMETER VEHICULAR USE AREA ADJACENT TO STREETS: WHERE POSSIBLE, PARKING LOTS SHALL RECEIVE ONE (1) TREE PER 30 LF OF STREET

10' WIDE LANDSCAPE BUFFER. 4.1. LOTS A, C, & J SURFACE PARKING LOTS SHALL DEVIATE FROM THE LANDSCAPE ORDINANCE, AND WILL NOT RECEIVE THE ABOVE LANDSCAPE

BUFFER, TO MAXIMIZE PARKING STALL COUNTS. 4.2. LOT F SHALL RECEIVE PERIMETER LANDSCAPING ALONG SANTA FE ONLY. 88-425-06 INTERIOR VEHICULAR USE AREA: WHERE POSSIBLE, PARKING LOTS

SHALL RECEIVE 35 SF OF LANDSCAPE AREA PER EVERY PARKING SPACE, ONE (1) TREE PER FIVE (5) PARKING SPACES, AND ONE (1) SHRUB PER PARKING SPACE. 5.1. LOTS A, C, F, & J SURFACE PARKING LOTS SHALL DEVIATE FROM THE LANDSCAPE ORDINANCE, AND WILL NOT RECEIVE THE ABOVE LANDSCAPE

AREAS, TO MAXIMIZE PARKING STALL COUNTS. 88-425-07 PARKING GARAGE SCREENING: WHERE POSSIBLE, PARKING GARAGES

88-425-08 MECHANICAL/UTILITY EQUIPMENT SCREENING: WHERE POSSIBLE, ALL EQUIPMENT SHALL BE SCREENED WITH DENSELY PLANTED BUFFER.

8. <u>88-425-09 OUTDOOR USE SCREENING:</u> WHERE POSSIBLE, ALL OUTDOOR WORK AREAS SHALL BE SCREENED WITH DENSELY PLANTED BUFFER.

LANDSCAPE GENERAL NOTES

1. FINAL LANDSCAPE PLANS WILL BE PROVIDED IN THE FINAL DEVELOPMENT PHASE OF THE PROJECT.

2. ALL WORK SHALL CONFORM TO ALL APPLICABLE STANDARDS AND SPECIFICATIONS OF THE CITY OF KANSAS CITY, MISSOURI IN CURRENT USAGE. ALL STANDARDS NOT COVERED BY THE CITY SHALL BE APWA STANDARDS IN CURRENT USAGE UNLESS OTHERWISE NOTED.

ALL NEW PLANTING AREAS TO RECEIVE 100% IRRIGATION COVERAGE PER SPECIFICATIONS AND SHALL BE INSTALLED AND OPERATIONAL PRIOR TO INSTALLATION OF ANY LANDSCAPE MATERIAL.

CONTRACTOR SHALL VERIFY EXACT LOCATION OF ALL EXISTING UTILITIES, DRAIN LINES AND IRRIGATION PIPING PRIOR TO COMMENCING WORK AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES, DRAIN LINES AND IRRIGATION

5. CONTRACTOR SHALL VERIFY AND COORDINATE ALL FINAL GRADES WITH LANDSCAPE ARCHITECT PRIOR TO COMPLETION.

6. DEBRIS SHALL NOT BE ALLOWED TO ACCUMULATE AND SHALL BE REMOVED AT FREQUENT INTERVALS. AT COMPLETION OF WORK IN EACH AREA, THE CONTRACTOR SHALL GATHER AND REMOVE ALL DEBRIS, EQUIPMENT, AND EXCESS MATERIAL FROM THAT AREA. AT FINAL COMPLETION OF ALL WORK THE CONTRACTOR SHALL REMOVE ALL SUCH ITEMS FROM THE PREMISES.

7. LOCATION AND PLACEMENT OF ALL PLANT MATERIAL SHALL BE COORDINATED WITH LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.

8. THE LANDSCAPE PLANTING PLAN GRAPHICALLY ILLUSTRATES OVERALL PLANT MASSINGS. EACH PLANT SPECIES SHALL BE PLACED IN THE FIELD TO UTILIZE THE GREATEST COVERAGE OF THE GROUND PLANE. PLANT SPACING TO BE ADJUSTED IN FIELD TO AVOID UTILITIES, FOOTINGS, AND INLETS. THE FOLLOWING APPLIES FOR INDIVIDUAL PLANTINGS:

-ALL EVERGREEN SHRUBS AND CREEPING GROUNDCOVERS SHALL BE MINIMUM OF 2' FROM ANY PAVING EDGE. -ALL PLANTS OF THE SAME SPECIES SHALL BE EQUALLY SPACED AND SITED FOR THE BEST AESTHETIC VIEWING. -ALL TREES, EVERGREEN OR DECIDUOUS, SHALL BE A MINIMUM OF 4' FROM ANY PAVING EDGE.

9. ANY SUBSTITUTION OF SPECIFIED PLANT MATERIAL WILL NOT BE ALLOWED WITHOUT WRITTEN AUTHORIZATION FROM LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.

10. MULCH ALL PLANTING AREAS TO A DEPTH OF 3" DEPTH ACCORDING TO PLANS AND SPECIFICATIONS. SAMPLES SHALL BE APPROVED BY LANDSCAPE

11. ALL PLANT MATERIAL WILL BE HEALTHY, VIGOROUS AND FREE OF DISEASE AND INSECTS PER AAN STANDARDS. LANDSCAPE ARCHITECT RESERVES THE RIGHT TO REJECT ANY INFERIOR OR OTHERWISE UNSUITABLE PLANT MATERIAL PROPOSED FOR USE ON THE PROJECT.

12. ALL PLANTING BEDS NOT FULLY CONTAINED BY CONCRETE CURBS OR WALKS

13. PLANTS AND LANDSCAPE MATERIALS SHALL BE INSTALLED AS DETAILED ON

14. PLANT BACKFILL FOR TREES AND SHRUBS SHALL BE PER SPECIFICATIONS.

15. ALL PLANTING BEDS SHALL BE TREATED WITH DACTHAL PRE-EMERGENT HERBICIDE AT MANUFACTURER RECOMMENDED RATES AND SHALL BE COVERED WITH SPECIFIED MULCH APPLICATION. APPLY LIGHTER APPLICATION OF DACTHAL HERBICIDE TO TOP OF MULCH LAYER.

16. ALL AREAS DISTURBED DURING CONSTRUCTION THAT ARE NOT DESIGNATED AS PLANTING BEDS OR PAVEMENT AREAS SHALL BE SEEDED WITH A TURF TYPE BERMUDA GRASS PER SPECIFICATIONS.

17. ALL PLANT MATERIAL SHALL BE GUARANTEED FOR A PERIOD OF ONE YEAR AFTER OWNER'S FINAL ACCEPTANCE OF FINISHED JOB. ALL DEAD AND DAMAGED PLANT MATERIAL SHALL BE REPLACED BY LANDSCAPE CONTRACTOR AT THEIR EXPENSE. LANDSCAPE CONTRACTOR SHALL MAINTAIN PLANT MATERIAL UNTIL FINAL ACCEPTANCE.

18. ALL LANDSCAPE BEDS SHALL BE MOUNDED AS SHOWN ON PLANS AND DETAILS. 19. LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ACTUAL PLANT QUANTITIES REQUIRED TO COMPLETE THE PROJECT AS SHOWN ON THE

20. BUILDING FOOTPRINTS SHOWN ARE PRELIMINARY AND ARE SUBJECT TO CHANGE WITH FINAL DEVELOPMENT PLANS.

LANDSCAPE LEGEND

STREET TREE (DECIDUOUS) PARKING LOT TREE (DECIDUOUS)

OPEN SPACE AMENITY AREA

AS ALLOWED BY DX ZONING AND WILL BE SUBMITTED FOR FINAL PLANS APPROVAL PRIOR TO CONSTRUCTION.

EACH BUILDING PROJECT WILL CONFORM TO THE PERMITTED USE

ALL IMPROVEMENTS TO STREETSCAPE, SIDEWALKS, PLAZAS AND PEN SPACE AMENITY AREAS WILL BE DESIGNED IN ACCORDANCE WITH THE RECOMMENDATION OF THE WEST BOTTOMS STREETSCAPE AND WAYFINDING PLAN

CONCEPT PLANS ARE PROVIDED WITH THE ANTICIPATION OF UBSEQUENT PROJECT PLAN APPROVAL FOR EACH PARCEL PRIOR TO CONSTRUCTION. EACH NEW BUILDING WILL FOLLOW THE DESIGN GUIDELINES SET FOR IN THE GREATER DOWNTOWN AREA PLAN DEVELOPMENT GUIDELINES

SomeraRoad

West Bottoms Redevelopmen^a

West Bottoms, Kansas City, MO

Prepared For Somera Road Kansas City, MO 64101



300 West 22nd Street Kansas City, MO 64108 USA t +1 816 472 3360 f +1 816 472 2100

Consultant Name

SK DESIGN GROUP, LNC. 4600 COLLEGE BLVD SUITE 100 OVERLAND PARK, KS 66211 MASTER PLAN ARCHITECT S9 ARCHITECTURE

CIVIL ENGINEER

322 8TH AVENUE NEW YORK, NY 10001

BKV GROUP 222 NORTH 2ND STREET, SUITE 101 MINNEAPOLIS, MN 55401

MEP ENGINEER LANKFORD FENDLER + ASSOCIATES 1730 WALNUT KANSAS CITY, MO 64108

No. Description

PRELIMINARY LANDSCAPE PLAN

L001

PRELIMINARY O	VERSTORY PLANT SCHEDULE			
DECIDUOUS TREES	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
AMM	ACER MIYABEI 'MORTON' TM	STATE STREET MIYABEI MAPLE	3" CAL.	B&B
ATP	ACER TRUNCATUM X PLATANOIDES 'WARRENRED' TM	PACIFIC SUNSET MAPLE	3" CAL.	B&B
GBA	GINKGO BILOBA 'AUTUMN GOLD' TM	AUTUMN GOLD MAIDENHAIR TREE	3" CAL.	B&B
GDE	GYMNOCLADUS DIOICA 'ESPRESSO'	KENTUCKY COFFEETREE	3" CAL.	B&B
NSW	NYSSA SYLVATICA 'WILDFIRE'	WILDFIRE BLACK GUM	3" CAL.	B&B
OV	OSTRYA VIRGINIANA	AMERICAN HOPHORNBEAM	3" CAL.	B&B
PAM	PLATANUS X ACERIFOLIA 'MORTON CIRCLE' TM	EXCLAMATION! LONDON PLANE TREE	3" CAL.	B&B
PTN	POPULUS TREMULOIDES 'NE ARB' TM	PRAIRIE GOLD QUAKING ASPEN	3" CAL.	B&B
QB	QUERCUS BICOLOR	SWAMP WHITE OARK	3" CAL.	B&B
QS	QUERCUS SHUMARDII	SHUMARD OAK	3" CAL.	B&B
TDM	TAXODIUM DISTICHUM 'MICKELSON' TM	SHAWNEE BRAVE BALD CYPRESS	3" CAL.	B&B
	•	•		•
ORNAMENTAL TREES	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
AGA	AMELANCHIER X GRANDIFLORA 'AUTUMN BRILLIANCE'	AUTUMN BRILLIANCE APPLE SERVICEBERRY	2" CAL.	B&B
CCT	CERCIS CANADENSIS TEXENSIS 'OKLAHOMA'	OKLAHOMA TEXAS REDBUD	2" CAL.	B&B
CV	CHIONANTHUS VIRGINICUS	WHITE FRINGETREE	2" CAL.	B&B
HID	HAMAMELIS X INTERMEDIA 'DIANE'	DIANE WITCH HAZEL	2" CAL.	B&B
МВ	MAGNOLIA X 'BUTTERFLIES'	BUTTERFLIES MAGNOLIA	2" CAL.	B&B

			DDEL INJINIA DV LIN	IDERSTORY PLANT SCHEDULE				
	SIZE	CONTAINER	DECIDUOUS SHRUBS	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER	SPACIN
	3" CAL.	B&B	HQS	HYDRANGEA QUERCIFOLIA 'SIKE'S DWARF'	SIKE'S DWARF OAKLEAF HYDRANGEA		CONTAINER	
	3" CAL.	B&B		SPACING PER PLAN			O O I VI I VII VII VII VII VII VII VII V	
 E	3" CAL.	B&B	PMS	PHILADELPHUS X 'MINIATURE SNOWFLAKE'	MINIATURE SNOWFLAKE MOCKORANGE	5 GAL.	CONTAINER	
	3" CAL.	B&B	POS	SPACING PER PLAN PHYSOCARPUS OPULIFOLIUS 'SMPOTW' TM	TINY WINE NINEBARK	5 GAL	CONTAINER	
	3" CAL.	B&B	103	SPACING PER PLAN	TINT WINE NINEDARK	J GAL.	CONTAINER	
	3" CAL.	B&B	RTB	RHUS TYPHINA 'BAILTIGER' TM	TIGER EYES STAGHORN SUMAC	3` HT.	CONTAINER	
REE	3" CAL.	B&B		SPACING PER PLAN	OMOOTH BOOF		CONTAINED	
	3" CAL.	B&B	RB	ROSA BLANDA SPACING PER PLAN	SMOOTH ROSE	5 GAL.	CONTAINER	
	3" CAL.	B&B	VC	VIBURNUM CARLESII	KOREANSPICE VIBURNUM	5 GAL.	CONTAINER	
	3" CAL.	B&B		SPACING PER PLAN			'	
<u> </u>	3" CAL.	B&B	WFA	WEIGELA FLORIDA 'ALEXANDRA' TM SPACING PER PLAN	WINE & ROSES WEIGELA	5 GAL.	CONTAINER	
				OFACING FER FLAN				<u> </u>
	SIZE	CONTAINER	ORNAMENTAL GRASSES	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER	SPACIN
VICEBERRY	2" CAL.	B&B	BGB	BOUTELOUA GRACILIS 'BLONDE AMBITION'	BLONDE AMBITION BLUE GRAMA		CONTAINER	ļ
	2" CAL.	B&B	PN	PANICUM VIRGATUM 'NORTHWIND'	NORTHWIND SWITCH GRASS		CONTAINER	
	2" CAL.	B&B	SSC	SCHIZACHYRIUM SCOPARIUM 'CAROUSEL'	CAROUSEL LITTLE BLUESTEM		CONTAINER	ļ
	2" CAL.	B&B		OTHER CONTROL OF THE OF	O. W.O O O C L EIT I L L D L O L O I L IVI			1.5 0.0.
	2" CAL.	B&B	PERENNIALS	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER	SPACIN
			AM	ALLIUM X 'MILLENIUM'	MILLENIUM ORNAMENTAL ONION		CONTAINER	ļ
			Al	ASCLEPIAS INCARNATA	SWAMP MILKWEED	1 GAL.	CONTAINER	ļ
			AOO	ASTER OBLONGIFOLIUS 'OCTOBER SKIES'	OCTOBER SKIES FALL ASTER		CONTAINER	
			BSF	BAPTISIA X 'SOLAR FLARE' TM	SOLAR FLARE PRAIRIEBLUES FALSE INDIGO		CONTAINER	ļ
			CNN	CALAMINTHA NEPETA NEPETA	LESSER CALAMINT		CONTAINER	ļ
			CVZ	COREOPSIS VERTICILLATA 'ZAGREB'	ZAGREB TICKSEED		CONTAINER	ļ
			EM				CONTAINER	ļ
				ECHINACEA Y IVELLOW MAY DARLING TM	MAGNUS PURPLE CONEFLOWER			1
			EYM	ECHINACEA X 'YELLOW MY DARLING' TM	COLOR CODED YELLOW MY DARLING CONEFLOWER		CONTAINER	ļ
			EP	EUPHORBIA POLYCHROMA	CUSHION SPURGE		CONTAINER	ļ
			HG	HOSTA X "GUACAMOLE"	GUACAMOLE HOSTA		CONTAINER	ļ
			HH	HOSTA X 'HALCYON'	HALCYON HOSTA		CONTAINER	
			MOC	MONARDA X 'OUDOLF'S CHARM'	OUDOLF'S CHARM BEE BALM		CONTAINER	ļ
			NCP	NEPETA X 'CAT'S PAJAMAS'	CAT'S PAJAMAS CATMINT		CONTAINER	ļ
			PAD	PEROVSKIA ATRIPLICIFOLIA `DENIM `N LACE`	DENIM `N LACE RUSSIAN SAGE		CONTAINER	ļ
			POV	POLYGONATUM ODORATUM 'VARIEGATUM'	VARIEGATED SOLOMON'S SEAL		CONTAINER	ļ
			RPE	RODGERSIA PINNATA 'ELEGANS'	ELEGANT RODGERSIA		CONTAINER	ļ
			RFS	RUDBECKIA FULGIDA SULLIVANTII 'LITTLE GOLDSTAR'	LITTLE GOLDSTAR CONEFLOWER		CONTAINER	
			SNM	SALVIA NEMOROSA 'MAY NIGHT'	MAY NIGHT SAGE	1 GAL.	CONTAINER	ļ
			SAJ	SEDUM X 'AUTUMN JOY'	AUTUMN JOY SEDUM		CONTAINER	
			SMH	STACHYS MONIERI 'HUMMELO'	COMMON BETONY	1 GAL.	CONTAINER	18" O.C
							Т.	Ι.
			ORNAMENTAL GRASS AREAS	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER	SPACIN
			CD	CAREX DIVULSA	GRASSLAND SEDGE	1 GAL.	CONTAINER	18" o c
			CE	CAREX EBURNEA	BRISTLELEAF SEDGE		CONTAINER	
			HM	HAKONECHLOA MACRA	JAPANESE FOREST GRASS		CONTAINER	ļ
			JE	JUNCUS EFFUSUS	COMMON RUSH		CONTAINER	ļ
			JIB	JUNCUS INFLEXUS 'BLUE ARROWS'	BLUE ARROWS JUNCUS		CONTAINER	ļ
			SA	SESLERIA AUTUMNALIS	AUTUMN MOOR GRASS		CONTAINER	ļ
		LL CONFORM TO THE PERMITTED	USE	OLULINA AU I UIVIINALIU	AUTUIVIN IVIOUN GNASS	GAL.	LOONTAINER	12 0.6.
		AND WILL BE SUBMITTED FOR FINE PRIOR TO CONSTRUCTION.		BOTANICAL NAME	COMMON NAME	0175	CONTAINER	SDACIN
		- "	PERENNIAL AREAS			SIZE		ļ
		EETSCAPE, SIDEWALKS, PLAZAS A		AMSONIA HUBRICHTII	ARKANSAS BLUESTAR		CONTAINER	ļ
OPEN SPACE AM	IENITY AREAS	S WILL BE DESIGNED IN ACCORDA DATION OF THE WEST BOTTOMS	ANCE AIS	AMSONIA TABERNAEMONTANA 'STORM CLOUD'	STROM CLOUD EASTERN BLUESTAR		CONTAINER	ļ
		AND WAYFINDING PLAN	AC	ASARUM CANADENSE	WILD GINGER		CONTAINER	
			BMJ	BRUNNERA MACROPHYLLA 'JACK FROST' TM	JACK FROST SIBERIAN BUGLOSS	1 GAL.	CONTAINER	112" o.c.
						+.	+ ·	
CONCEPT PL		DVIDED WITH THE ANTICIPATION C N APPROVAL FOR EACH PARCEL P	OF GSM	GERANIUM SANGUINEUM 'MAX FREI' HEUCHERA RICHARDSONII	MAX FREI BLOODRED GERANIUM PRAIRIE ALUM ROOT		CONTAINER CONTAINER	24" o.c.

PRELIMINARY O	VERSTORY PLANT SCHEDULE			
DECIDUOUS TREES	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
AMM	ACER MIYABEI 'MORTON' TM	STATE STREET MIYABEI MAPLE	3" CAL.	B&B
ATP	ACER TRUNCATUM X PLATANOIDES 'WARRENRED' TM	PACIFIC SUNSET MAPLE	3" CAL.	B&B
GBA	GINKGO BILOBA 'AUTUMN GOLD' TM	AUTUMN GOLD MAIDENHAIR TREE	3" CAL.	B&B
GDE	GYMNOCLADUS DIOICA 'ESPRESSO'	KENTUCKY COFFEETREE	3" CAL.	B&B
NSW	NYSSA SYLVATICA 'WILDFIRE'	WILDFIRE BLACK GUM	3" CAL.	B&B
OV	OSTRYA VIRGINIANA	AMERICAN HOPHORNBEAM	3" CAL.	B&B
PAM	PLATANUS X ACERIFOLIA 'MORTON CIRCLE' TM	EXCLAMATION! LONDON PLANE TREE	3" CAL.	B&B
PTN	POPULUS TREMULOIDES 'NE ARB' TM	PRAIRIE GOLD QUAKING ASPEN	3" CAL.	B&B
QB	QUERCUS BICOLOR	SWAMP WHITE OARK	3" CAL.	B&B
QS	QUERCUS SHUMARDII	SHUMARD OAK	3" CAL.	B&B
TDM	TAXODIUM DISTICHUM 'MICKELSON' TM	SHAWNEE BRAVE BALD CYPRESS	3" CAL.	B&B
	·	·		
ORNAMENTAL TREES	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
AGA	AMELANCHIER X GRANDIFLORA 'AUTUMN BRILLIANCE'	AUTUMN BRILLIANCE APPLE SERVICEBERRY	2" CAL.	B&B
ССТ	CERCIS CANADENSIS TEXENSIS 'OKLAHOMA'	OKLAHOMA TEXAS REDBUD	2" CAL.	B&B
CV	CHIONANTHUS VIRGINICUS	WHITE FRINGETREE	2" CAL.	B&B
HID	HAMAMELIS X INTERMEDIA 'DIANE'	DIANE WITCH HAZEL	2" CAL.	B&B
МВ	MAGNOLIA X 'BUTTERFLIES'	BUTTERFLIES MAGNOLIA	2" CAL.	B&B

SomeraRoad

Project
West Bottoms Redevelopment

West Bottoms, Kansas City, MO

Prepared For Somera Road Kansas City, MO 64101



300 West 22nd Street Kansas City, MO 64108 USA t +1 816 472 3360 f+1 816 472 2100

Consultant Name CIVIL ENGINEER SK DESIGN GROUP, LNC. 4600 COLLEGE BLVD SUITE 100 OVERLAND PARK, KS 66211 MASTER PLAN ARCHITECT S9 ARCHITECTURE 322 8TH AVENUE NEW YORK, NY 10001

ASSOCIATE ARCHITECT BKV GROUP 222 NORTH 2ND STREET, SUITE 101 MINNEAPOLIS, MN 55401

MEP ENGINEER LANKFORD FENDLER + ASSOCIATES 1730 WALNUT KANSAS CITY, MO 64108

No. Description

1 PRELIMINARY LANDSCAPE SCHEDULES

Sheet Number

PRELIMINARY

LANDSCAPE SCHEDULES

2th ST CONSTRUCTION COST ESTIMATION BY LINEAR FT.

AL PROBABLE COST per Linear Ft......\$ 1,61

otal Probable Cost is an estimate of professional services and construction services of the corridor.

See Appendix for more detailed cost breakdown.

TYPICAL STREET SECTION: pedestrian focused mall

See pg 58-67 for public-realm materials & amenities

Opt 1: salvaged cobble pavers:

match pattern to historic equivalent Opt 2: Flex use zone cobble pavers only:

~10' FLEXIBLE USE , 12' LIMITED VEHICLE

match cobble match cobble stormwater capture area backfill vault

12TH ST

See pg 58-67 for public-realm materials & amenities specifications (including pedestrian lighting option)

Provide roll-over curbs adjacent to loading zones

Stormwater Capture / Infiltration Zone in blue

Opt 2: Parking lane cobble pavers only:

standard asphaltic concrete in drive lane

PARKING LANE 11' DRIVE LANE 11' DRIVE LANE PARKING LANE 7' SIDEW

Parking zone uses: Parking

Adjacent business us

~19' FLEXIBLE USE

WEST BOTTOMS STREETSCAPE PLAN

Historic Core Alley Improvements through Stormwater Management

Many alleys, primarily in the Historic Warehouse Distirct core, are in need of infrastructure improvements. Existing alleys suffer from a lack of proper drainage, lighting, consistent pavement, and accessability. Adjacent private property spatially contiguous to alley R.O.W. need to be included in alley infrastructure upgrades, requiring a public/private partnership. Water Services Department has earmarked funding to explore public/private partnerships in the West Bottoms that provide added value to property owners and capture stormwater.

Alley Infrastructure Recommendations:

- Provide basic infrastructure including proper stormwater drainage, pavement, lighting, utility consolidation, and improved functional accessibility to buildings. 2. Follow recommendations spelled out in Greater Downtown Area Plan
- development guidelines regarding access, screening, setbacks, etc. Where appropriate or feasible, utilize alleys as public space.







Example updated alley

WEST BOTTOMS STREETSCAPE PLAN



12th Street Streetscape Design Intent

12th Street pedestrian mall streetscape is designed to give significant priority to the pedestrain rather than the vehicle. This is accomplished by allowing for free movement for pedestrians while limiting movement for vehicles through design of vertical elements (such as bollards, street trees, planters, amenities) and traffic slowing features (such as speed tables, serpentine drive paths, signage). Stakeholders have already reclaimed the roadway during warehouse weekends and festivals: this streetscape design intends to formalize their actions.

12th Street Pedestrian Mall primary focus corridor priorities are as follows: Complete a traffic study at the intersection of 12th Street & Liberty Street and 12th Street & Hickory Street to determine if safety concerns warrant signalization, traffic redirection, or other design solutions.

Design and implement a pedestrian-focused right-of-way north of 12 Street Viaduct on 12th Street that limits vehicular traffic and accentuates the adjacent outdoor spaces next to the Viaduct (1150 lin. ft.)

Streetscape design elements must match or complement other improvements occuring along Hickory Street corridor (p. 32-33)

4. Provide amenities and signage beneath the Viaduct deck at the intersection of Hickory to direct people arriving to the West Bottoms and accommodate the existing events and gatherings occurring there

↑ Typical Cobble Street example with concrete sidewalks & amenities

Hickory Street will serve as the Historic Warehouse District's pedestrian focused

livable street north-south spine. Stakeholders want to preserve the district's historic

character but allow the land use to become mixed residential, retail, and light

industrial; the streetscape will embody that desire by carefully allowing existing

historic elements to remain when possible. Simple, authentic, durable materials are chosen to help frame the district's architectural character. The historic cobble street

surface must adhere to Department of Public Works requirements. If not possible,

consider cobble for parking areas and alternative permeable pavement solutions

Upgrade pedestrian realm experience in coordination with green

3. Finish streetscape implementation beyond green infrastructure project limits

4. Streetscape design elements must match or complement other improvements

5. Preserve Hickory Street R.O.W. south through the Liberty Street Parking Lot

occuring along 12th Street corridor (p. 24-25) and 11th Street corridor

Address safety concerns for pedestrians at the railroad crossing where the

crossing arm and building pass within inches when lowered into operation

Return Hickory Street pavement to original cobble surface where feasible

infrastructure improvements occuring between 12th St and 14th St (1020

Hickory Street primary focus carridor priorities are as follows:

from 9th Street to 12th Street (1190 lin. ft.)

to connect to 16th Street in the Stockyards District

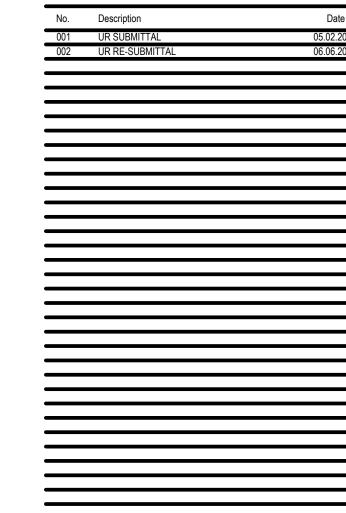
Hickory Streetscape Design Intent

Develop a street design for Liberty between 12th and 11th continuing to the

east along 11th Street that solves existing safety and circulation issues

WEST BOTTOMS STREETSCAPE PLAN

NOT FOR CONSTRUCTION



STREETSCAPE TYPICAL

West Bottoms

SomeraRoad

West Bottoms, Kansas City, MO

Redevelopment

Prepared For Somera Road Kansas City, MO 64101



300 West 22nd Street Kansas City, MO 64108 USA t +1 816 472 3360 f +1 816 472 2100

Consultant Name CIVIL ENGINEER SK DESIGN GROUP, LNC. 4600 COLLEGE BLVD SUITE 100 OVERLAND PARK, KS 66211

MASTER PLAN ARCHITECT S9 ARCHITECTURE 322 8TH AVENUE NEW YORK, NY 10001 **BKV GROUP**

MINNEAPOLIS, MN 55401 MEP ENGINEER LANKFORD FENDLER + ASSOCIATES 1730 WALNUT KANSAS CITY, MO 64108

222 NORTH 2ND STREET, SUITE 101

SECTIONS

Sheet Number

TAL PROBABLE COST per Linear Ft......\$ 1,14 Total Probable Cost is an estimate of professional services and construction services of the corridor. See Appendix for more detailed cost breakdown. TYPICAL STREET SECTION: Pedestrian amenities focus corridor with maximized parking Standard asphaltic concrete 8 PARALLEL N 1 7 20' PARKING 90° ____ 70' ROW ____ DBABLE CONSTRUCTION COST......\$ 1

WEST BOTTOMS STREETSCAPE PLAN

See pg 58-67 for public-realm materials & amenities specifications (including pedestrian lighting option)

Stormwater Capture / Infiltration Zone in blue

Opt 2: Parking lane cobble pavers only

Provide roll-over curbs Parking zone uses:

11 DRIVE LANE 11 DRIVE LANE PARKING LANE 7' SIDE

Adjacent business us

11th ST CONSTRUCTION COST ESTIMATION BY LINEAR FT

TAL PROBABLE COST per Linear Ft...... \$ 1,42

ICKORY ST CONSTRUCTION COST ESTIMATION BY LINEAR

TAL PROBABLE COST per Linear Ft......\$ 1, Total Probable Cost is an estimate of professional services and construction services of the corridor.

TYPICAL STREET SECTION: Pedestrian amenities focus corridor

See Appendix for more detailed cost breakdown.

See Appendix for more detailed cost breakdown.

Brick Surface

al Probable Cost is an estimate of professional services and construction services of the corridor.

11th Street is the only street in the West Bottoms that does not have any street lights, and also lacks curbs and sidewalks, yet is one of the more trafficed routes within the historic core. It is a important east-west connection north of 12th Street that sees a significant amount of vehicular and pedestrian traffic, especially on Warehouse Weekends. Compared to other streets, 11th contains a number of undeveloped parcels that are easily accessible for parking because of a lack of curbs that prevent easy access. The streetscape design for 11th Street will accommodate a number of on-street parking spaces, provide access to existing temporary parking lots, define space for pedestrian movement, and allow for future street level retail infill. The street upgrades will improve access and safety within the district. 11th Street corridor priorities are: 1. Provide streetscape infrastructure including pavements, street lights, curbs, sidewalks (1480 lin. ft.) Provide 90 degree on-street parking on south end of R.O.W. 11th Street corridor extends south along Liberty to 12th Street: the corridor's LEGEND carrying capacity will depend on traffic study solutions identified at 12th Brick Surface Street and Liberty intersection Cobble Parking 4. Allow access to existing properties, including undeveloped parcels currently Cobble Street used for event parking while also providing a defined edge that accommodates Crosswalk vehicular and pedestrian movement O Street Trees Street Landscape 5. Streetscape design elements must match or complement other improvements

11th Street Streetscape Design Intent







Hickory Streetscape Design Intent

Hickory Street will serve as the Historic Warehouse District's pedestrian focused livable street north-south spine. Stakeholders want to preserve the district's historic character but allow the land use to become mixed residential, retail, and light industrial; the streetscape will embody that desire by carefully allowing existing historic elements to remain when possible. Simple, authentic, durable materials are chosen to help frame the district's architectural character. The historic cobble street surface must adhere to Department of Public Works requirements. If not possible, consider cobble for parking areas and alternative permeable pavement solutions for roadway.

Hickory Street primary focus corridor priorities are as follows: Return Hickory Street pavement to original cobble surface where feasible Upgrade pedestrian realm experience in coordination with green infrastructure improvements occuring between 12th St and 14th St (1020

lin. ft.) Finish streetscape implementation beyond green infrastructure project limits from 9th Street to 12th Street (1190 lin. ft.) 4. Streetscape design elements must match or complement other improvements

(p. 38-39) 5. Preserve Hickory Street R.O.W. south through the Liberty Street Parking Lot to connect to 16th Street in the Stockyards District

6. Address safety concerns for pedestrians at the railroad crossing where the crossing arm and building pass within inches when lowered into operation

occuring along 12th Street corridor (p. 24-25) and 11th Street corridor

Brick Surface Cobble Parking Cobble Street Crosswalk Street Trees Street Landscape Existing Building Potential New Green Space Existing Green Space Public/Private Partnership



WEST BOTTOMS STREETSCAPE PLAN

AL PROBABLE COST per Linear Ft......\$ 1,4

Total Probable Cost is an estimate of professional services and construction services of the corridor.

TYPICAL STREET SECTION: Pedestrian amenities focus corridor

See Appendix for more detailed cost breakdown.

WEST BOTTOMS STREETSCAPE PLAN

WEST BOTTOMS STREETSCAPE PLAN

LEGEND

Brick Surface

Cobble Parking

Cobble Street

Street Landscape

Existing Building

Pub/Priv Partner-

ship Opportunity

●●● Street Trees

L003

PLAN DEVELOPMENT GUIDELINES

SUBSEQUENT PROJECT PLAN APPROVAL FOR EACH PARCEL PRIOR TO CONSTRUCTION. EACH NEW BUILDING WILL FOLLOW THE DESIGN GUIDELINES SET FOR IN THE GREATER DOWNTOWN AREA

EACH BUILDING PROJECT WILL CONFORM TO THE PERMITTED USE

AS ALLOWED BY DX ZONING AND WILL BE SUBMITTED FOR FINAL

PLANS APPROVAL PRIOR TO CONSTRUCTION.

ALL IMPROVEMENTS TO STREETSCAPE, SIDEWALKS, PLAZAS AND

OPEN SPACE AMENITY AREAS WILL BE DESIGNED IN ACCORDANCE

STREETSCAPE AND WAYFINDING PLAN

WITH THE RECOMMENDATION OF THE WEST BOTTOMS

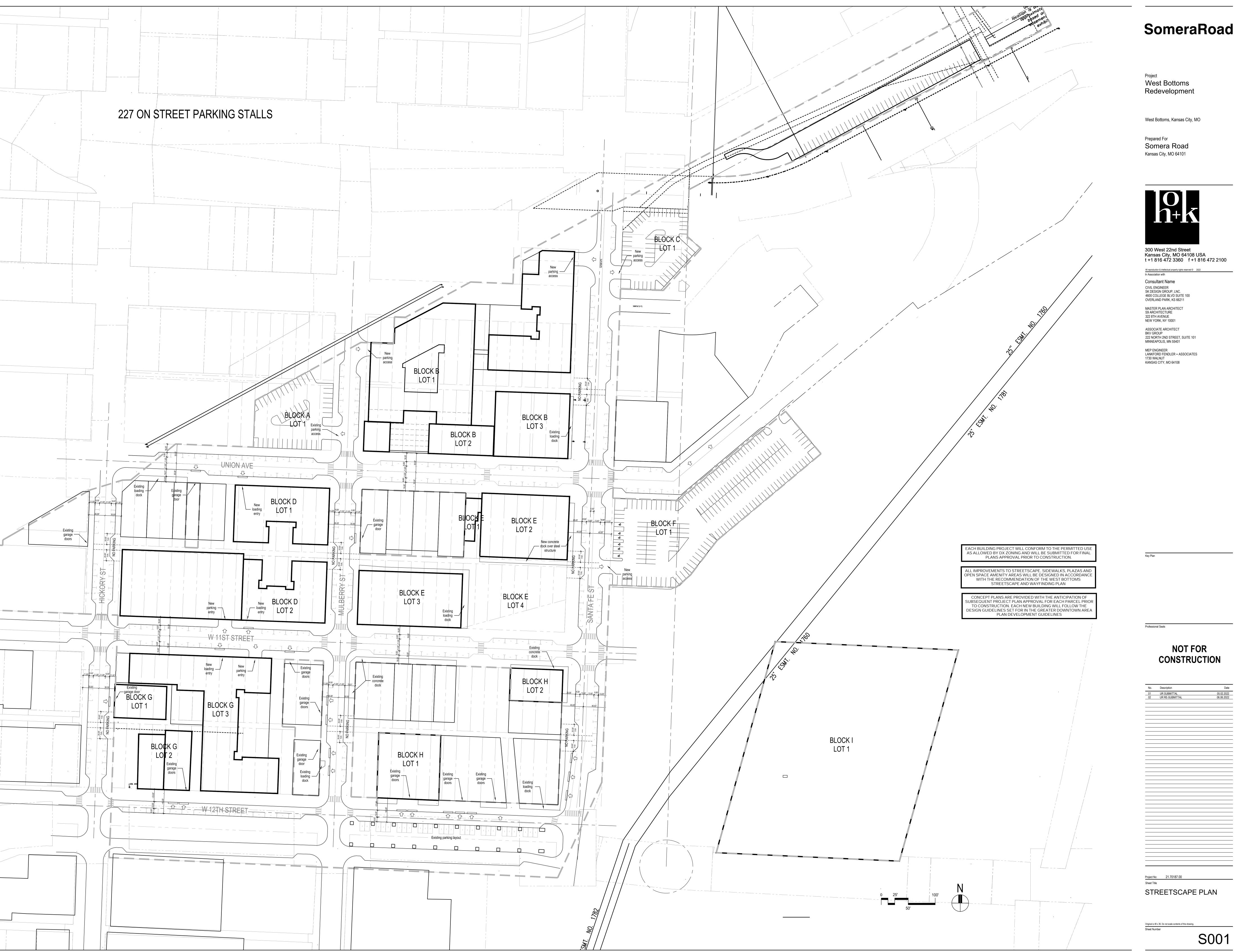
CONCEPT PLANS ARE PROVIDED WITH THE ANTICIPATION OF

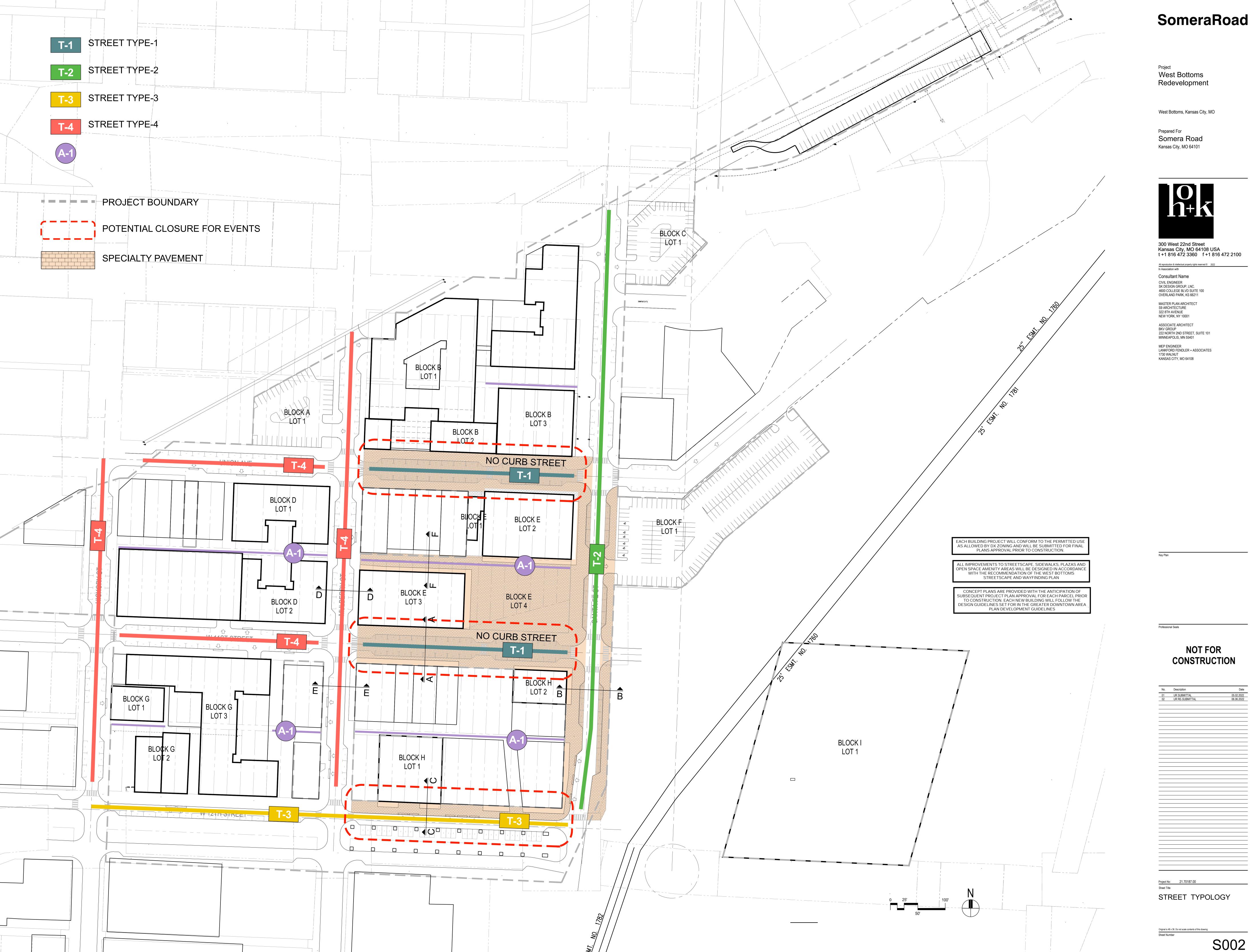
Cobble Parking Cobble Street Crosswalk Street Trees Street Landscape HICKORY R.O.W. Existing Building Potential New Green Space CITY PARKING Existing Green Space LOT STORMWATER Public/Private Partnership STREET PARK on half-blocks with 2 or STUDY AREA PROJECT AREA

WEST BOTTOMS STREETSCAPE PLAN

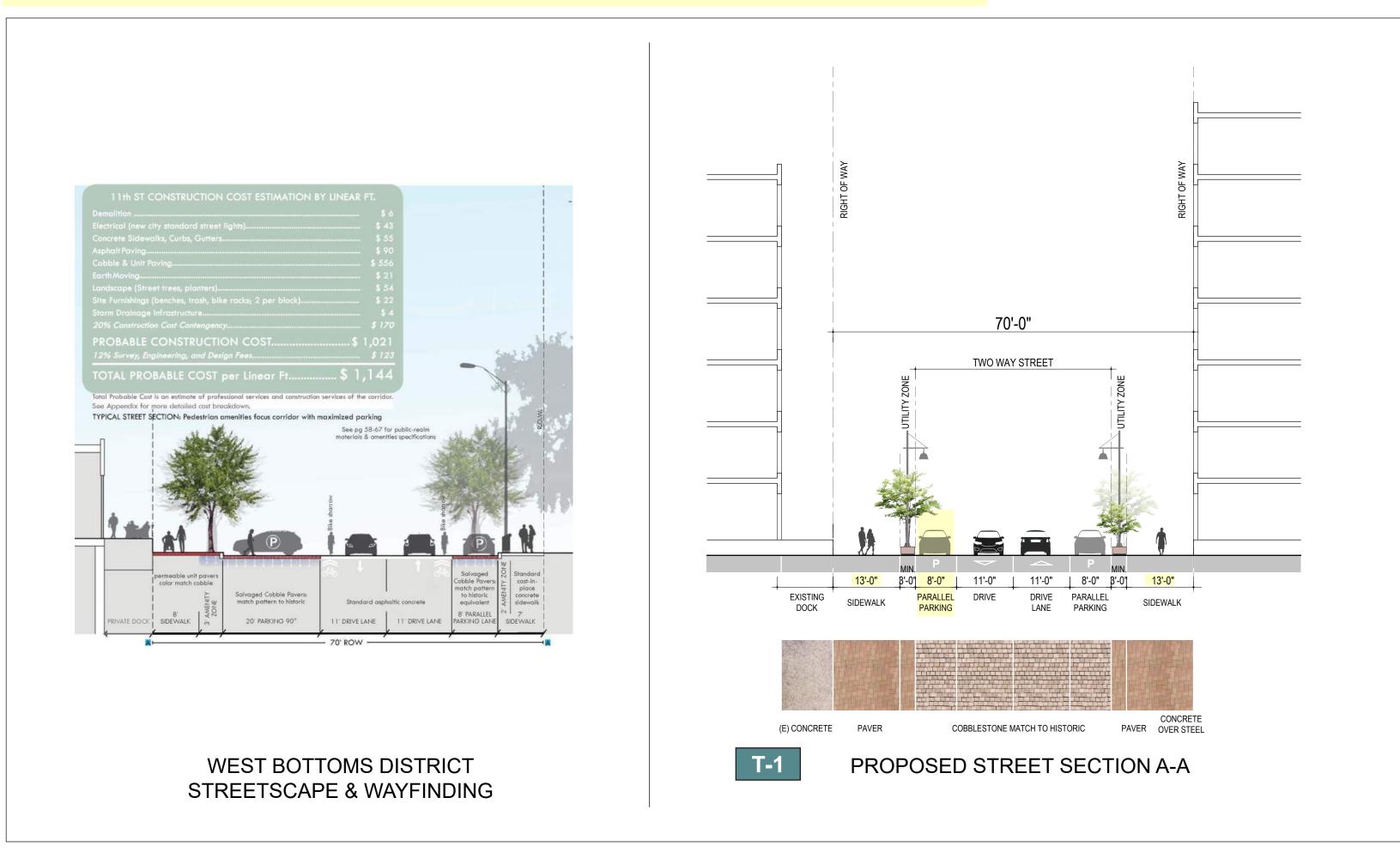
for roadway.

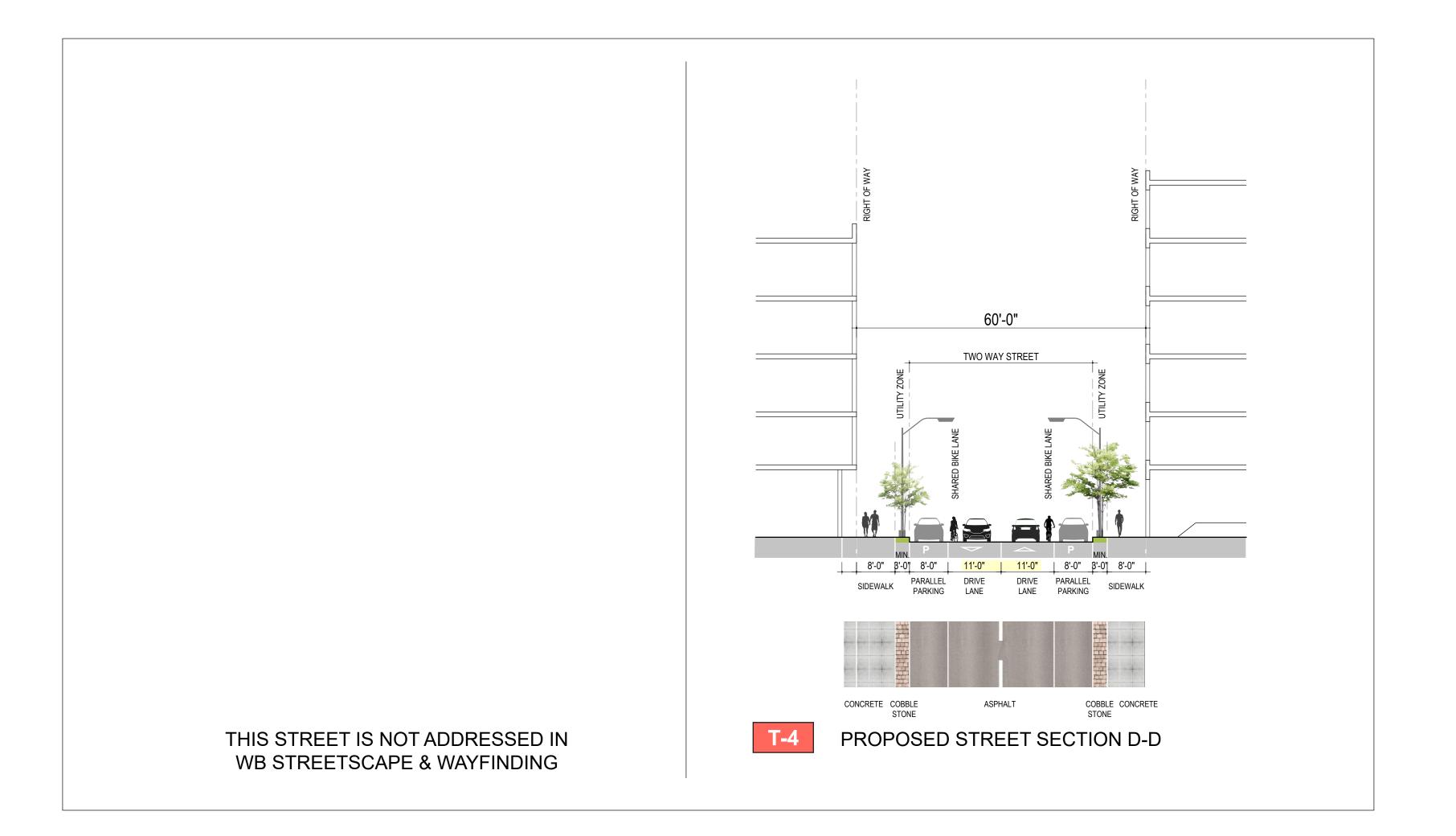
lin. ft.)

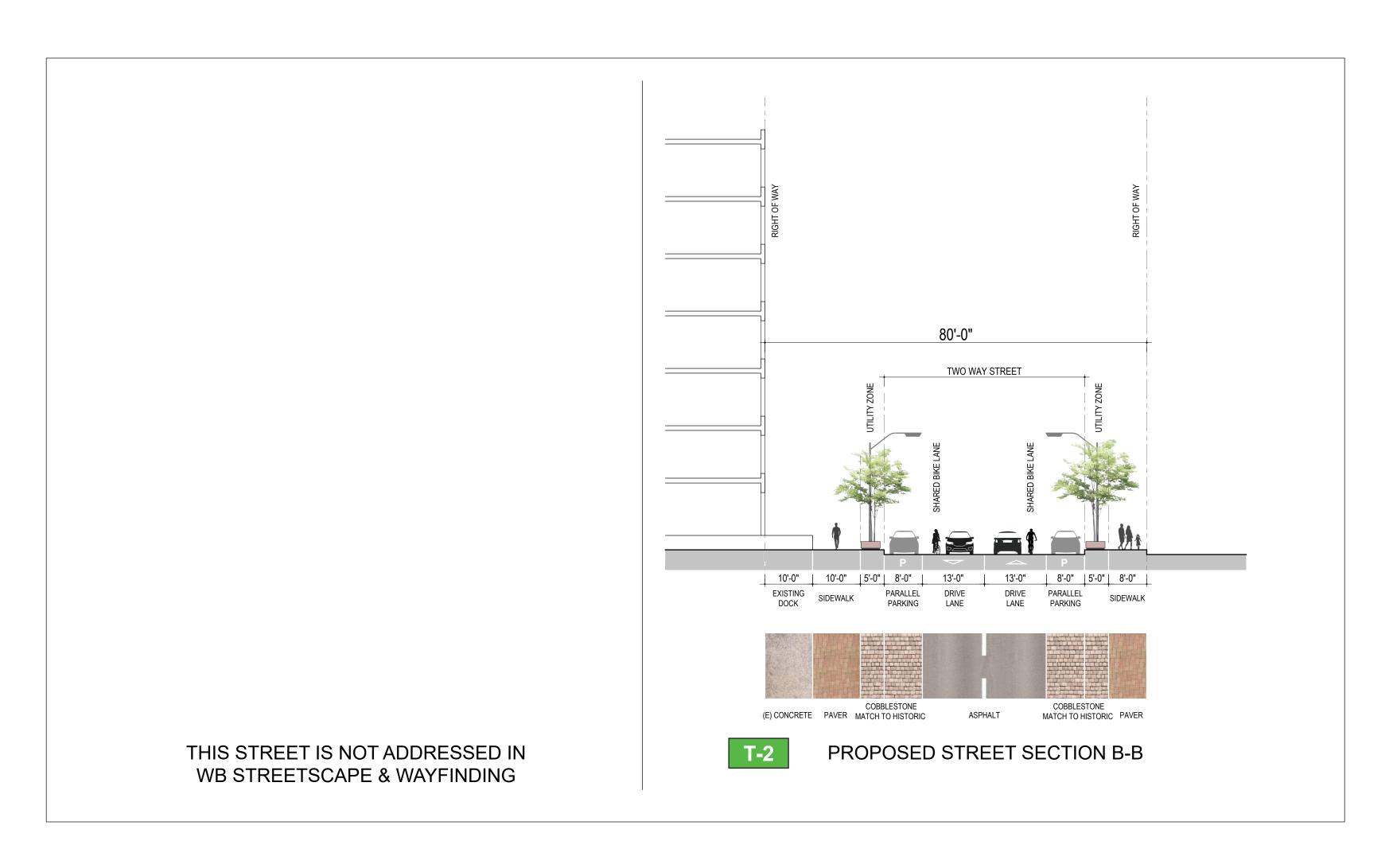


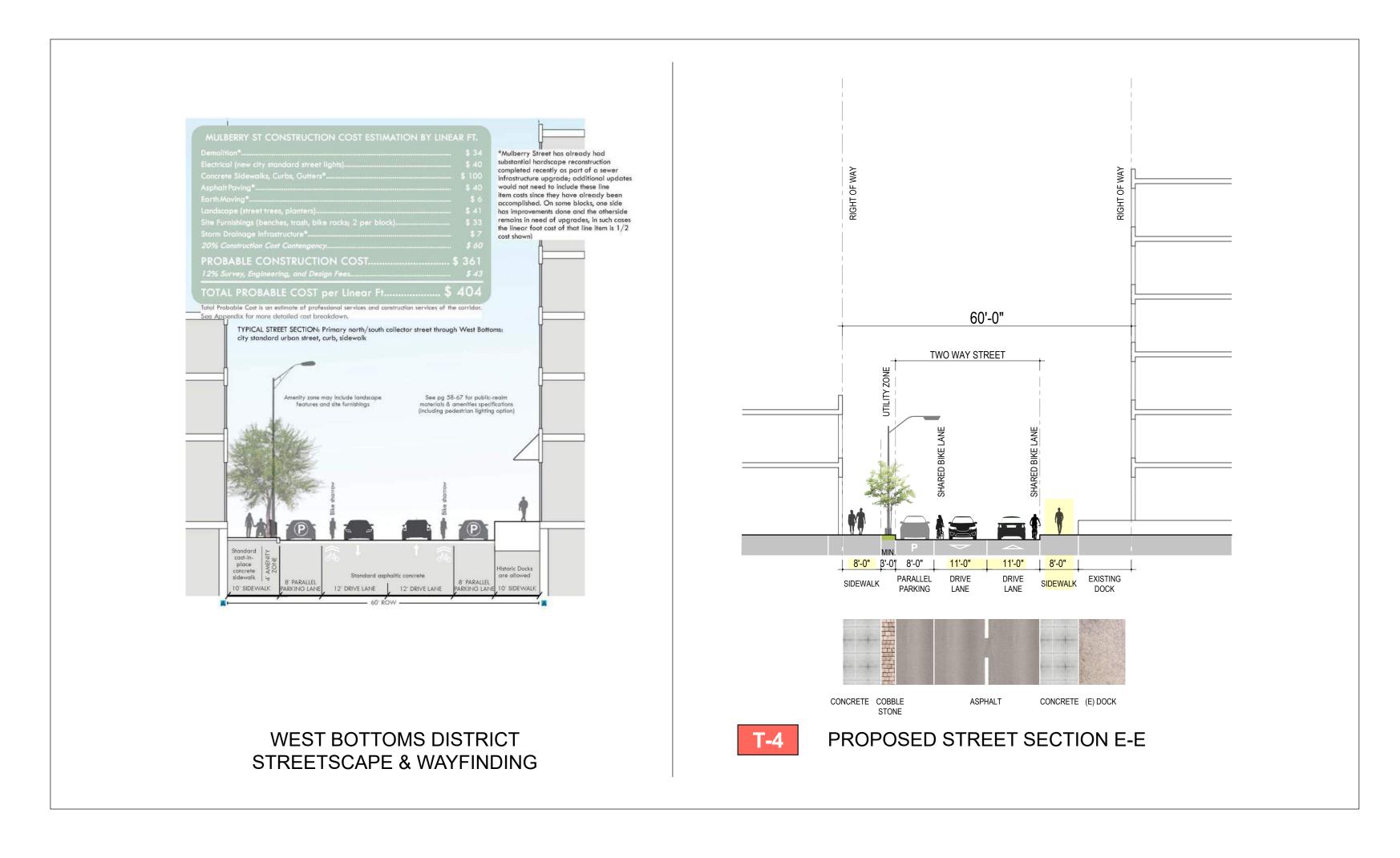


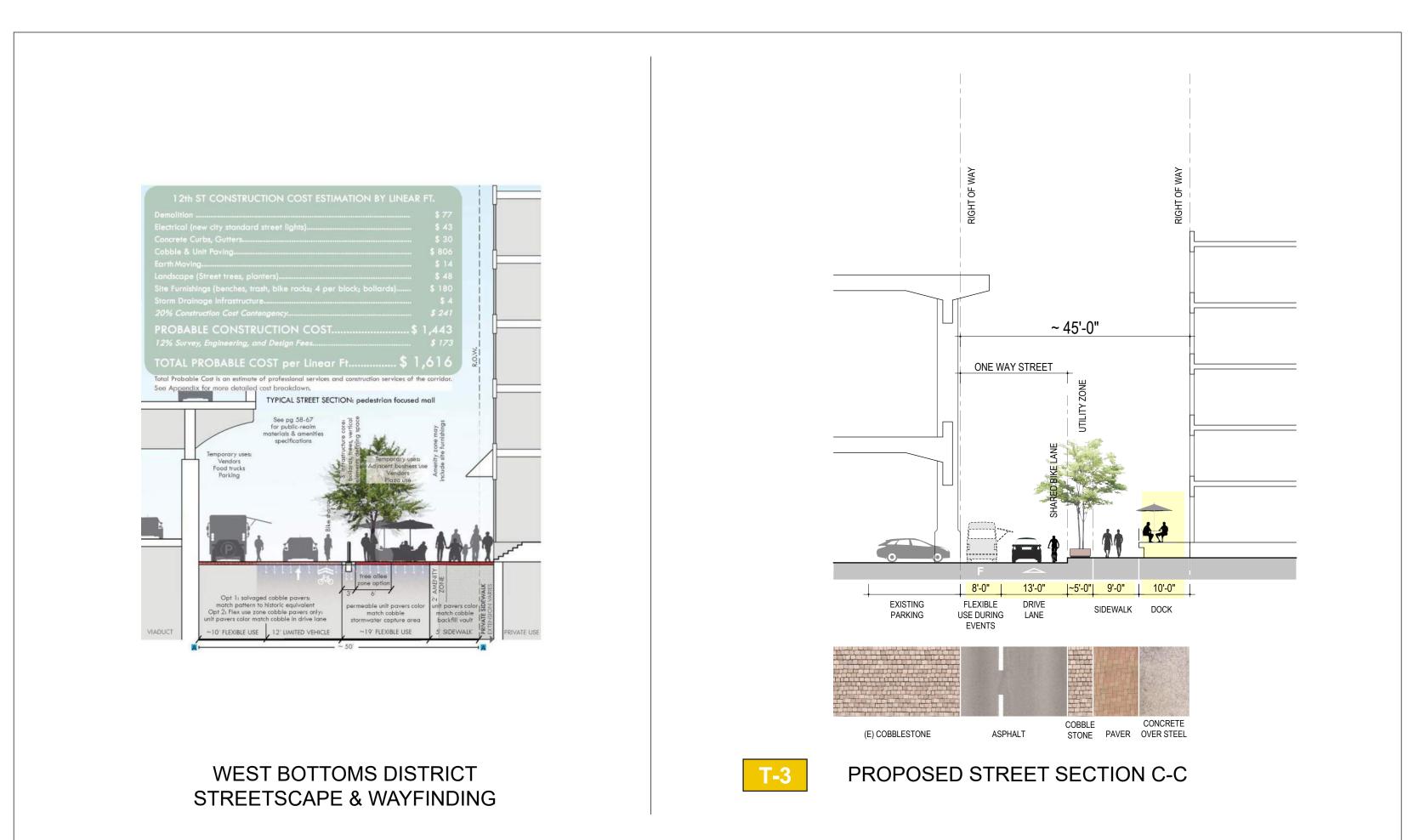
HIGHLIGHTED AREA DEVIATES FROM WEST BOTTOMS DISTRICT STREETSCAPE & WAYFINIDING

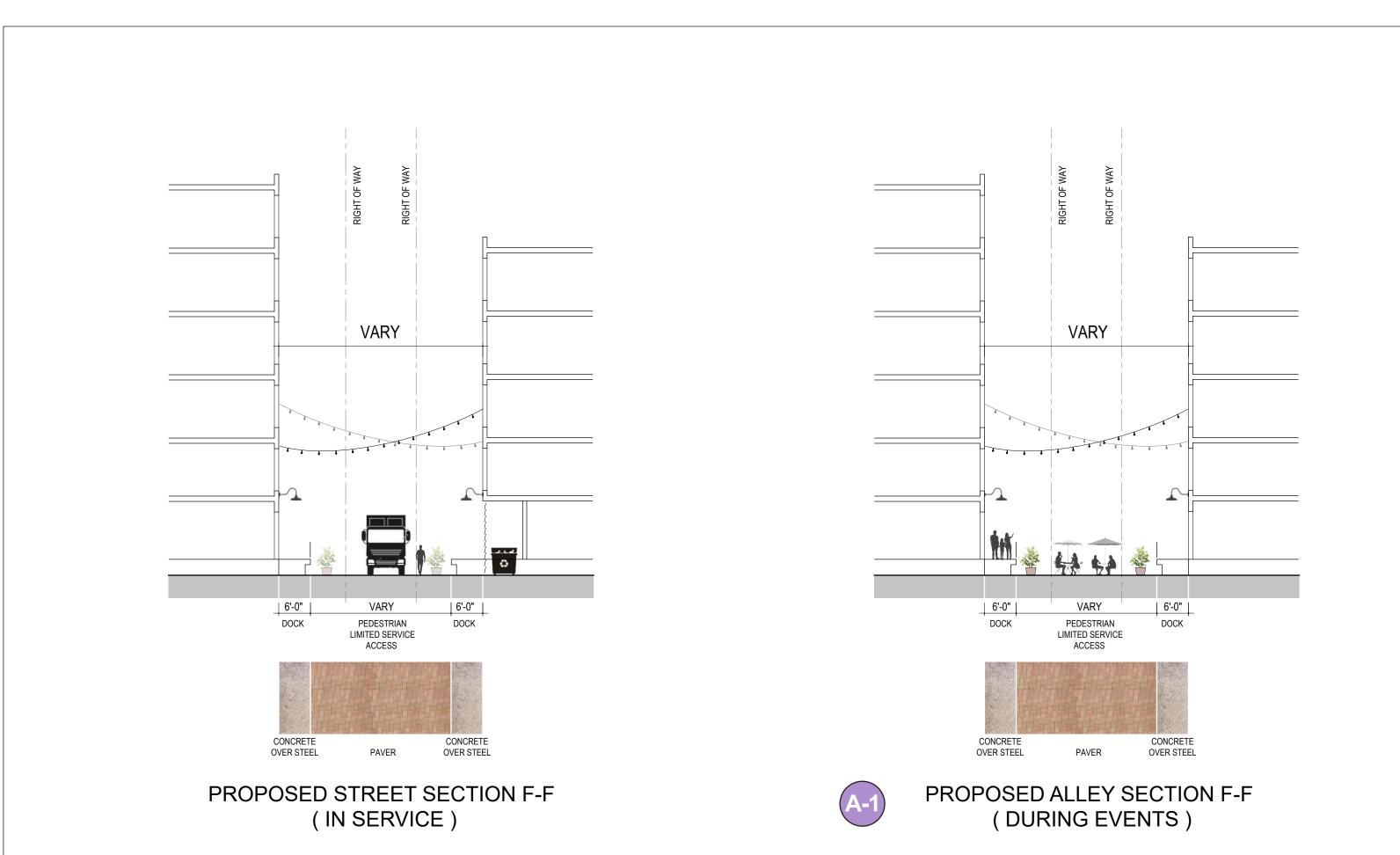












EACH BUILDING PROJECT WILL CONFORM TO THE PERMITTED USE AS ALLOWED BY DX ZONING AND WILL BE SUBMITTED FOR FINAL PLANS APPROVAL PRIOR TO CONSTRUCTION. ALL IMPROVEMENTS TO STREETSCAPE, SIDEWALKS, PLAZAS AND OPEN SPACE AMENITY AREAS WILL BE DESIGNED IN ACCORDANCE WITH THE RECOMMENDATION OF THE WEST BOTTOMS

CONCEPT PLANS ARE PROVIDED WITH THE ANTICIPATION OF SUBSEQUENT PROJECT PLAN APPROVAL FOR EACH PARCEL PRIOR TO CONSTRUCTION. EACH NEW BUILDING WILL FOLLOW THE DESIGN GUIDELINES SET FOR IN THE GREATER DOWNTOWN AREA PLAN DEVELOPMENT GUIDELINES

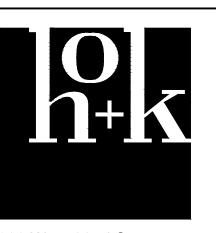
STREETSCAPE AND WAYFINDING PLAN

SomeraRoad

West Bottoms Redevelopment

West Bottoms, Kansas City, MO

Prepared For Somera Road Kansas City, MO 64101



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Consultant Name SK DESIGN GROUP, LNC. 4600 COLLEGE BLVD SUITE 100 OVERLAND PARK, KS 66211 MASTER PLAN ARCHITECT S9 ARCHITECTURE 322 8TH AVENUE

BKV GROUP 222 NORTH 2ND STREET, SUITE 101 MINNEAPOLIS, MN 55401

NEW YORK, NY 10001

MEP ENGINEER LANKFORD FENDLER + ASSOCIATES 1730 WALNUT KANSAS CITY, MO 64108

Professional Seals

No. Description

NOT FOR CONSTRUCTION

STREET

SECTIONS

Sheet Number

Original is 48 x 36. Do not scale contents of this drawing.

appendix C

DEVELOPMENT GUIDELINES

PURPOSE AND USE

These guidelines provide a framework for quality design that is consistent with the Greater Downtown Area Plan vision, goals and objectives and the principles in the FOCUS Urban Core Plan and the FOCUS Quality Places to Live and Work. These guidelines should be used in conjunction with the Land Use Plan and Building Height recommendations to provide a framework to shape future development in Greater Downtown.

RELATIONSHIP TO FIVE PLAN GOALS

- **Double the Population** By delineating and encouraging new areas of mixed use development (including residential).
- **Increase Employment** By encouraging and focusing density along the central business corridor and future transit corridor.

• **Create a Walkable Downtown** – The design guidelines ensure that pedestrian

amenities are provided and strive to produce the walkable urbanity to support public transit and 24-hour communities with broad economic bases. • Retain and Promote Safe, Authentic Neighborhoods – Urban design can have a major impact on safety. Recommendations focus on encouraging the design of defensible spaces and greater overall transparency between buildings and the street. Strategies that aim to increase pedestrian street activity will also have a

positive impact on perceived and actual safety downtown. Urban design improve-

ments should be designed to minimize the need for maintenance. Maintenance

should be shared by public and private entities and responsibilities should be

well-defined before construction. • **Promote Sustainability** – The guidelines encourage sustainable building practices which improve air and water quality, reduce energy consumption, and encourage alternative modes of transportation.

COMPONENTS

- This section includes two components the Urban Design Matrix, and the Development Guidelines.
- **Urban Design Matrix** The urban design matrix illustrates the ideal design elements based on district type (see diagrams) and street type. The Functional Use Diagrams reflect the general character of the various districts downtown.
- **Development Guidelines** The quidelines supplement the Functional Use Diagrams and provide more detail about the character, scale, design and layout of development projects.







GREATER DOWNTOWN AREA PLAN

ORIENTATION AND SETBACKS

(Refer to the Functional Use Diagrams)

extend the public realm including:

- Primary entrance enhancement

spaces (not the primary use).

public space is encouraged.

Provides street-level, pedestrian-oriented uses.

provide rhythm and interest along the street.

communicate the individual character of the area.

- Extension of the pedestrian realm

Outdoor café

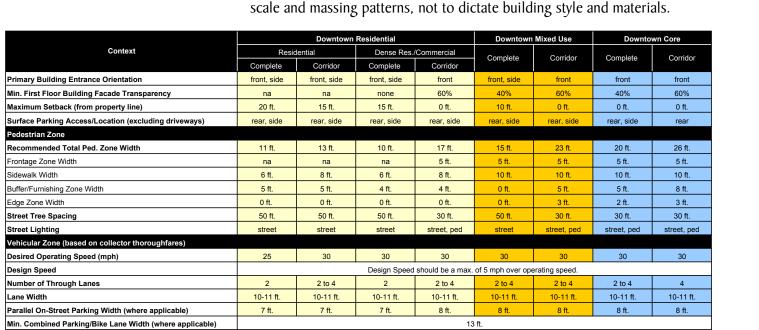
Sidewalk retail

- Public plaza

side buildings.

realm of the street.

URBAN DESIGN MATRIX The following Functional Use Diagrams illustrate the general elements that The Urban Design Matrix makes ideal design recommendations based on district and street type. The goal of the matrix is to influence the general future development should achieve and represent the most characteristic



Note: Angled parking may also be appropriate on certain streets.

Bike Lanes (preferred min. width) (where applicable

Medians (14' where applicable)



SEE S003 FOR STREET TYPOLOGY

• In the Downtown Core and some other mixed use areas, buildings should be built

to the property line. Buildings should define the street edge. Additional setback

may be considered for purposes that augment street level pedestrian activity and

- Landscaping which is complementary and accessory to pedestrian activity and public

- Where appropriate or feasible, the enhancement and utilization of alleys as

In mixed use areas buildings should maintain and reinforce street level pedestrian

- No more than 25% of any primary street frontage should be occupied by uses with no

Buildings should define a majority of the street edge. Surface parking lots,

On residential streets, buildings may be set back (see Functional Use Diagrams)

Buildings should have a primary entrance facing and directly accessible from the

lots in mixed use areas, building entrances are encouraged on both streets.

• Buildings are encouraged to have multiple entrances that open out to the public

public street, rather than oriented towards side or rear parking areas. For corner

New development should incorporate design elements and interpretive signage that

Large blank walls along streets should be avoided whenever possible. Where blank

comfort and interest. Some possible methods for treating blank walls include:

- Installing vertical trellis in front of the wall with climbing vines or plant materials.

In residential areas, garages should be located behind residences and accessed

Providing art over a substantial portion of the blank wall surface.

- Setting the wall back and providing a landscaped or raised planter in front of the wall.

- Dividing the mass of the wall to create a visual impression of a series of smaller buildings

walls are unavoidable they should receive design treatment to increase pedestrian

to allow for landscaped planting beds. For rowhouses, elements like stoops should

large courtyards, plazas and open space areas are encouraged behind or along

activity regardless of size or use. This might include a design that:

need for pedestrian traffic. Drive-through uses are discouraged.

- Maintains a continuous, transparent, highly permeable and active street wall.

Use AASHTO minimums as a target, but consider combinations of horizontal and vertical per AASHTO Green Book

Downtown Mixed Use districts contain commercial uses, but are most successful

Downtown Core districts represent the most dense urban environments. They are most successful when they contain a broad mix of commercial, residential, and other uses.

Guidelines

- The following design recommendations are supplemental to the Urban Design
- The guidelines are intended to be flexible. Not every guideline will apply for every
- the Decision Making Criteria (see Implementation Chapter). Application of the guidelines should respond to factors such as the scope of
- These guidelines are not intended to be all inclusive of acceptable materials and/

Create transitions between areas of different scales and intensities that are as

Transitions may be provided through use of complementary materials and architec-

Transitions between high scale buildings and low scale buildings may be achieved

Small green spaces, courtyards, squares, parks and plazas should be used whenever

A combination of landscaping, walls, fences and/or berms should be used where

Building elevations facing a less intensive use shall provide "finished" edges using

materials consistent with primary elevations and adjacent neighborhood.

• Developments should be designed to minimize ingress or egress from commercial

• Glare and spillage into adjacent properties should be kept to a minimum through

• All sidewalks and walkways in mixed use areas on Corridor Streets (or as specified

• The design of exterior light fixtures should be consistent throughout a develop-

• Architectural materials should complement the character of the existing built environ-

New downtown buildings should be designed in such a way that they don't appear

ment through use of high quality, durable materials. Suggested materials include brick,

wood, metal, glass, concrete, stone, stucco, cast stone, terracotta, tile and masonry.

- Care should be taken to avoid nostalgic reproductions and confusion of the historical

This guideline does not preclude consideration of the use of materials, scale or massing

found on older buildings. Preservation or restoration of original facade materials is desired.

- Buildings should be designed with a variety of scales, creating a scale and level of detail

- Clearly articulating different uses at lower building levels will aid in creating a sense of

in district streetscape plans) should have pedestrian level lighting.

of fixtures should be as consistent as possible along a single block.

High quality sustainable architectural materials are also encouraged.

Applied 'faux' facades or other inappropriate materials should be removed.

Encourage public art to be integrated into the building/site design.

at the street level appropriate to the pedestrian.

human scale in mid- and high-rise buildings.

Create buildings with human scale.

Non-urban construction materials, such as imitation masonry, or plywood are

ARCHITECTURAL CHARACTER AND MATERIALS

to have been built significantly earlier than they were.

the use of cut-off fixtures or other devices. Low noise level lights should be used.

ment, or within a district, but not throughout the planning area. The type and size

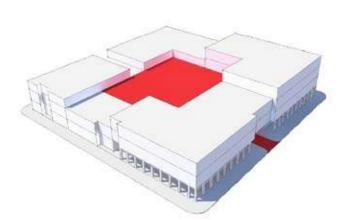
other transitions tools are not possible or not adequate, but should not mask areas





FENESTRATION

shade trees.



STRUCTURED PARKING

- Structured parking garages should be located on the interior or rear of the block surrounded by buildings whenever possible or at the zero lot line.
- When located along a street frontage, and where feasible, developments are encouraged to include first floor pedestrian active uses such as retail and services.

The exterior finish and architectural articulation should enhance the

- facade design, complement surrounding buildings and screen the parking area. Blank walls on parking structures are discouraged, particularly on Corridor Streets (see page 27).
- Parking structure façades should relate to the scale, proportion, and character of the district. Openings should be screened to obscure parked vehicles. Ramps and
- particularly on a facade with frontage on a primary street. • Where screening reduces visibility for "natural surveillance", other security measures

sloping floors should not be expressed on the outside of the building,

- should be provided. • Structured parking lots should incorporate green solutions where possible, that
- may include the following:
- Site Considerations: - Provide accommodations for bicycle storage, bus drop, etc.
- Allow preferred parking for car pools and hybrid vehicles. - Provide location for storm water filtration and infiltration from garage deck. - Provide street trees and other landscaping opportunities as native species. - Provide high albedo rating top deck surface.

Use energy efficient elevator equipment.

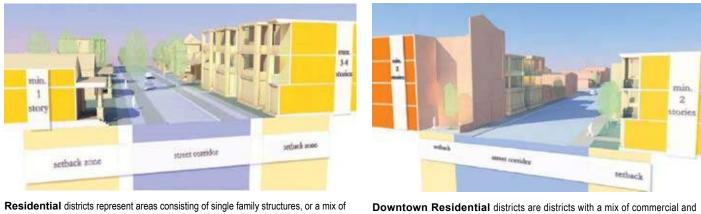
Consider use of renewable generation at the top deck.

Energy Considerations: - Use energy efficient lighting with daylight sensors, time clocks and possible motion - Create façade that provides for greater daylight penetration into the garage.





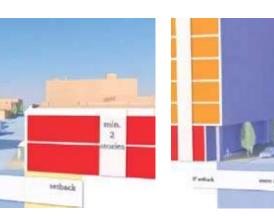
GREATER DOWNTOWN AREA PLAN



single family and smaller scale multi-plex units. Pockets of commercial uses and public residential structures. The residential units in these districts tend to be in multi-plex facilities such as schools are highly encouraged.

elements of each of the functional areas.

structures of various sizes, or in upper floors of commercial structures.



when incorporating many diverse uses.

TRANSITIONS

(See Building Height Map and Land Use Plan)

by gradually stepping height down.

seamless as possible and avoid abrupt transitions.

tural character, setbacks, scale, and orientation of buildings.

possible as a way to provide transitions between uses.

from view and decrease "natural surveillance".

projects into adjacent residential neighborhoods.

All new development should include a site lighting plan.

- Matrix and Diagrams and are intended to guide future development plans.
- project. However, as many quidelines as are practicable, feasible and applicable to the unique site characteristics should be incorporated into development plans. Exceptions to the guidelines should be weighed against the Vision Statement and
- the project (i.e. project size, new construction vs. renovation) and existing site
- or design features or to preclude or inhibit creative and eclectic ideas.

APPENDIX C



 Provide 'transparency' or openness to create a visual connection at the street level. The street level of commercial/mixed use structures should have a dominant trans-

On-Street parking should be preserved whenever possible.

with designated pedestrian pathways.

ing occupants for residential buildings.

SURFACE PARKING LOTS

quality soils.

(but not limited to) the following:

pavement and bio-retention swales.

If walls are utilized to screen

surface parking lots, materi-

als should complement the

associated building.

Multiple small parking lots

architectural character of the

are more desirable than single

large lots. Larger surface lots

should be subdivided with

landscaped islands including

Parking lots should include bicycle and scooter parking facilities and be designed

Encourage LEED guidelines for bike parking in publicly subsidized projects. Provide secure bike racks and/or storage as near as practicable to the building entrance for at least five percent of all building users for commercial or institutional buildings;

provide covered storage facilities for bicycles for at least 15 percent of the build-

Parking lot lighting and light from vehicles should not glare into adjacent prop-

erties. Consider light in access drive location. Vehicle entrances and pedestrian

Curb cuts for parking lots should be kept to a minimum. Access should be from

alleys (1st choice) or major arterials (2nd choice) instead of residential streets.

mixed use garage. Where this is not feasible, parking beside the building may be

appropriate but parking should comprise a small percentage of the street frontage

Additional surface parking lots in mixed use areas, particularly those with street

All surface parking lots are encouraged to incorporate Green solutions including

- Generous landscaped areas with plants suited to the urban landscape, adequate planting area and

Managing stormwater on-site utilizing BMPs including, but not limited to native plantings, porous

• At a minimum, all surface parking lots are encouraged to provide a combination of

perimeter and interior landscaping per the Downtown Surface Lot Design Guideline

requirements (Chapter 52). Provide direct, and safe pedestrian connections through

on the block. Where feasible, parking is encouraged to be in below grade structures

Parking should be located at the rear of the property behind buildings, or in a

(ensuring safety through both active and passive security measures).

entrances should be clearly marked and visible from the street.

- Windows at the street level of all buildings should be transparent. Reflective glass
- Building renovation projects are encouraged to restore windows to the original design and restore window openings that have been closed during past renovations.

AWNINGS AND CANOPIES

- Awnings and canopies are encouraged on non-residential facades and should include the following elements: - Be made of durable materials like an opaque fabric material, canvas or resin.
- Be a color compatible with the building façade. - Building signage may be integrated as part of the overall awning and canopy design. Be incorporated into the overall building design (including the supporting framework)
- and enhance the overall character of the area. Architectural elements that project from the building, such as building-mounted lighting, awnings, canopies and signage, should be designed so as to ensure pedestrian safety and comfort.

- All signs should be made of high quality and durable materials. The design of the sign should complement the character of the building or structure
- and the surrounding neighborhood. Signage should focus towards either vehicular or pedestrian traffic, not both. The design and scale of signage should be appropriate for the audience.
- Pedestrian-oriented signs, such as blade signs affixed perpendicular to the building or suspended from a canopy and artistic signs are encouraged.
- Signs should reinforce and enhance the neighborhood character not define it.
- Window signs that are painted or etched on display windows are encouraged and should preserve first floor transparency.
- Sandwich board signs, subject to City approval, may be appropriate for street
- Signs must not obscure important architectural features.
- New billboards, freestanding pole mounted signs and pylon signs are inappropriate with the character of Downtown and are strongly discouraged.
- Existing billboards should be removed whenever possible, particularly when tax incentives are requested.
- Blinking, flashing, neon, electronic or moving signs are discouraged. (Refer to the Public Realm Chapter for more recommendations relating to
- signs in public spaces)

APPENDIX C



• Small pedestrian scale blocks should be utilized in development projects. Large "superblocks" which degrade the existing street connections are discouraged.

- When large developments do occur, they should be designed to maintain pedestrian permeability. There should be at least one pedestrian-through connection every block (approximately every 600 feet). Pedestrian walkways and plazas should be clearly delineated or spatially sepa-
- lighting, landscaping, and special pavement treatments. • Alleys should be integrated with overall access and site circulation whenever possible. - Where buildings are built to the alley edge, consider opportunities for alley display windows and secondary customer or employee entries.

rated from parking and driveways through use of elements including bollards,

- Where intact, historic alley facades should be preserved along with original features and - Efforts to create public spaces out of alleys are encouraged. Provide convenient access for service and delivery vehicles without disrupting
- Provide direct, safe and convenient access to public transit facilities and integrate
- into the overall site design whenever possible. Buildings should have pedestrian entrances accessible directly from the adjacent

Ensure design that is accessible to all people including those with physical limi-

tations. All access improvements including sidewalks and crosswalks shall meet

the requirements of the Americans with Disabilities Act Accessibility Guidelines

- Curb cuts should be kept to a minimum within mixed use areas. Continuous curb cuts are not appropriate anywhere within the planning area.
- When commercial uses abut residential areas, there should be a pedestrian connection (public or private) from residential area to the commercial area at least

MASSING AND PROPORTION

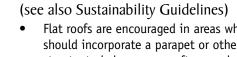
The massing and proportion of buildings should generally be consistent with the Functional Use Diagrams and with adjacent historic buildings.

- Significant departures in height and mass can be visually disruptive. Building proportions should strive for a cohesive rhythm.
- The design of buildings should respond to unique aspects of the site, such as prominent locations at the termini of key streets and view corridors, prominent locations on bluffs and overlooks, the relationship to nearby historic or landmark buildings, or corner locations.









• Flat roofs are encouraged in areas where it is the dominant roof type. Flat roofs

from an alley when possible.

- should incorporate a parapet or other screening device along facades facing public streets, to help screen rooftop mechanical systems. • Pitched roofs should only be used in areas where they are prevalent. Materials for pitched roofs are encouraged to incorporate a color that complements the general
- character of the building. Mansard or false roofs are discouraged. • Sustainable design techniques and materials such as green roofs are encouraged to reduce the amount of storm water runoff, enhance the local environment and

their visibility from taller buildings and from the street.

reduce energy costs. • Roofs should be designed and constructed in such a way that they acknowledge

SCREENING

- All screening should be designed to maintain visibility for "natural surveillance". • Screen storage, loading docks, and parking facilities and incorporate Crime Prevention Through Environmental Design (CPTED) principles in design. Mechanical and technology equipment should be screened from view from public
- Waste dumpsters and recycling receptacles should be screened on all sides, with a gate or door for access. The recommended enclosure should be complementary with the building facade materials and landscaped for additional screening. Shared waste
- dumpsters and recycling areas are encouraged. Any wall or fences shall be constructed of durable materials such as masonry, wrought iron or heavy wood. Plywood, chain link, and transparent materials are not
- Any lights or outdoor speakers should be arranged to reflect the light and transmit

SUSTAINABILITY

(See also Land Use recommendations)

the noise away from adjacent buildings (see Lighting Guidelines).

- LEED Certification or equivalent sustainable design is encouraged, particularly for public facilities and projects requesting incentives. Stormwater Management - Green Solutions and BMPs that achieve multiple benefits
- are encouraged. Examples include, but are not limited to: - Pervious Surfaces. - Rainwater Harvesting. - Landscaping and Street Trees.



- Promote and encourage building practices that effectively manage stormwater landscaping to improve air quality, etc.).
- are encouraged. Examples include, but are not limited to: - Providing alternative energy production. - Employing efficient design practices, utilize efficient heating and cooling technology,
- and proper solar orientation.
- Minimize impervious hard surfaces and provide trees and landscaping. Consider green roof or light color of roof to reduce heat.
- Transportation and Mobility Encourage transit, biking and walking.
- Encourage development projects requesting incentives to provide public spaces.



AS ALLOWED BY DX ZONING AND WILL BE SUBMITTED FOR FINAL PLANS APPROVAL PRIOR TO CONSTRUCTION. ALL IMPROVEMENTS TO STREETSCAPE, SIDEWALKS, PLAZAS AN

EACH BUILDING PROJECT WILL CONFORM TO THE PERMITTED US

PEN SPACE AMENITY AREAS WILL BE DESIGNED IN ACCORDANCE

WITH THE RECOMMENDATION OF THE WEST BOTTOMS STREETSCAPE AND WAYFINDING PLAN

CONCEPT PLANS ARE PROVIDED WITH THE ANTICIPATION OF SUBSEQUENT PROJECT PLAN APPROVAL FOR EACH PARCEL PRIOI

TO CONSTRUCTION. EACH NEW BUILDING WILL FOLLOW THE DESIGN GUIDELINES SET FOR IN THE GREATER DOWNTOWN AREA PLAN DEVELOPMENT GUIDELINES

HIGHLIGHTED AREA DEVIATES FROM WEST BOTTOMS DISTRICT STREETSCAPE & WAYFINIDING

Original is 48 x 36. Do not scale contents of this drawing. Sheet Number

BUILDING STANDARDS

SomeraRoad

West Bottoms

Redevelopmen

West Bottoms, Kansas City, MO

Prepared For

Somera Road

Kansas City, MO 64101

300 West 22nd Street

Consultant Name

SK DESIGN GROUP, LNC. 4600 COLLEGE BLVD SUITE 100 OVERLAND PARK, KS 66211

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222 NORTH 2ND STREET, SUITE 101

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MEP ENGINEER

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Energy - Energy efficient design and measures to reduce energy consumption

 Materials - "Sustainable" materials are encouraged (see Building Materials). Reduce Heat Island and improve air quality.

Waste - Provide opportunities for recycling/composting.

APPENDIX C











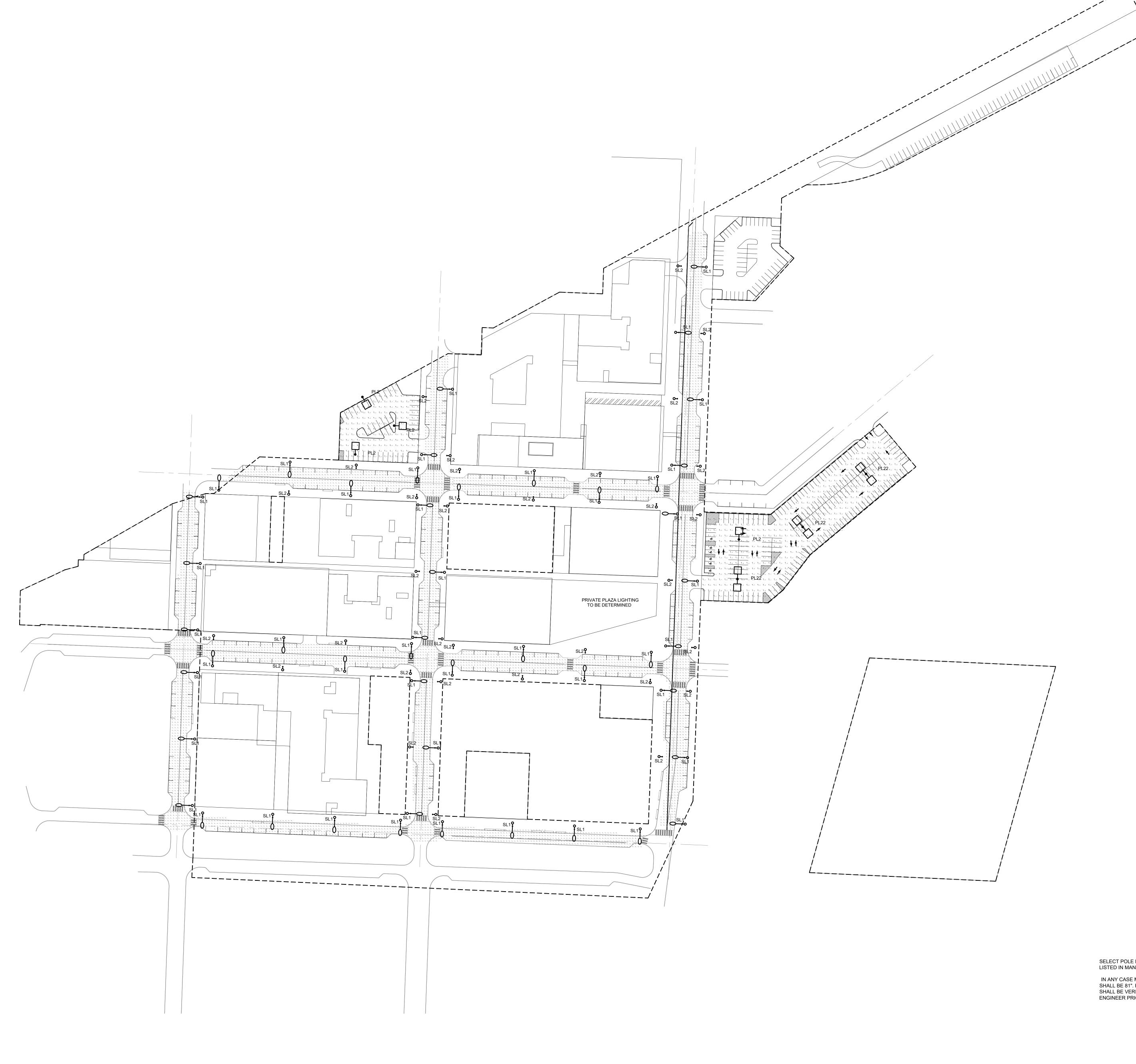












SITE PLAN - ELECTRICAL NECESCALE: 1" = 70'-0"

Label	Calc Type	Units	Avg	Max	Min	Avg/Min	Max/N
1000 Hickory	Illuminance	FC	2.89 fc	7.2 fc	1.3 fc	2.22 fc	5.5
1000 Mulberry	Illuminance	FC	3.04 fc	7.6 fc	1.4 fc	2.17 fc	5.4
1000 Santa Fe	Illuminance	FC	3.12 fc	7.6 fc	1.3 fc	2.40 fc	5.8
1100 Hickory	Illuminance	FC	2.79 fc	7.1 fc	1.4 fc	1.99 fc	5.0
1100 Mulberry	Illuminance	FC	3.10 fc	7.7 fc	1.4 fc	2.21 fc	5.5
1100 Santa Fe	Illuminance	FC	3.11 fc	7.5 fc	1.0 fc	3.11 fc	7.5
11th and Hickory Int	Illuminance	FC	4.41 fc	7.3 fc	1.0 fc	4.41 fc	7.3
11th and Mulberry Int	Illuminance	FC	7.00 fc	10.6 fc	4.5 fc	1.56 fc	2.3
11th and Santa Fe Int	Illuminance	FC	4.57 fc	7.2 fc	1.7 fc	2.69 fc	4.2
12th and Hickory Int	Illuminance	FC	3.89 fc	7.1 fc	0.7 fc	5.56 fc	10.
12th and Mulberry Int	Illuminance	FC	6.24 fc	8.6 fc	3.7 fc	1.69 fc	2.3
9000 Santa Fe	Illuminance	FC	3.05 fc	7.4 fc	0.6 fc	5.08 fc	12.
E. 11th Street	Illuminance	FC	2.99 fc	7.2 fc	1.3 fc	2.30 fc	5.5
E. 12th Street	Illuminance	FC	3.36 fc	8.2 fc	1.4 fc	2.40 fc	5.8
E. Union	Illuminance	FC	2.98 fc	7.1 fc	1.3 fc	2.29 fc	5.4
East Parking Lot	Illuminance	FC	2.76 fc	18.4 fc	0.5 fc	5.52 fc	36.
N.W. Parking	Illuminance	FC	2.79 fc	8.5 fc	0.7 fc	3.99 fc	12.
Union and Mulberry Int	Illuminance	FC	6.56 fc	9.4 fc	4.3 fc	1.53 fc	2.1
Union and Santa Fe Int	Illuminance	FC	4.03 fc	7.5 fc	1.0 fc	4.03 fc	7.5
W. 11th Street	Illuminance	FC	3.00 fc	7.6 fc	1.3 fc	2.31 fc	5.8
W. 12th Street	Illuminance	FC	3.47 fc	8.4 fc	1.5 fc	2.31 fc	5.6
W. Union	Illuminance	FC	2.94 fc	7.1 fc	1.2 fc	2.45 fc	5.9

SELECT POLE BASED ON MAXIMUM EPA LIGHTING STANDARD— LISTED IN MANUFACTURERS CATALOG. /— HANDHOLE WITH GASKET GROUNDING LUG BONDED TO IN ANY CASE MIN. POLE BASE DEPTH POLE INTERIOR NEAR HANDHOLE— GROUND WIRE, #8 SHALL BE 81". FINAL DEPTH OF POLE BASE SHALL BE VERIFIED WITH STRUCTURAL ENGINEER PRIOR TO PLACEMENT. CONDUIT BUSHING BEVEL EDGES— ANCHOR BOLTS - SIZE, CAST ALUMINUM BASE COVER NUMBER, AND PLACEMENT PER MFG. RECOMMENDATIONS —— FINISH PARKING SURFACE OR GRADE (6) #6 VERTICAL REINFORCING RODS EQUALLY SPACED *POLE BASE SHALL BE CALCULATED USING CONDUIT AND THE FOLLOWING CRITERIA: CONDUCTORS AS #8 BARE CU STRANDED SHOWN ON PLAN-GROUND CONDUCTOR <u>WWD LOADING</u> 80MPH (104 GUST) COPPER CLAD GROUND ROD 3/4" DIA. X 10'-0" LONG 24" DIA. DRILLED CONCRETE BASE— TOTAL EPA OF LUMINARIES +TOTAL EPA OF BRACKETS
=TOTAL EPA OF LUMINAIRES/BRACKETS #4 TIES AT 1'-0" CENTERS 6" LAP MIN—

LIGHTING POLE BASE DETAIL

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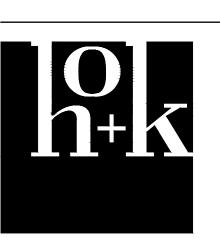
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West Bottoms
Redevelopment

West Bottoms, Kansas City, MO

Prepared For Somera Road Kansas City, MO 64101



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MASTER PLAN ARCHITECT S9 ARCHITECTURE 322 8TH AVENUE NEW YORK, NY 10001

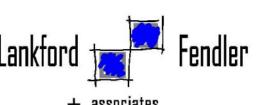
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MEP ENGINEER
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+ BSSCIBLES

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COA No. 2006001168

GREGORY FENDLER

NUMBER
PE-2006037230

 No.
 Description
 Date

 01
 UR RE-SUBMITTAL
 05.02.2022

 02
 UR RE-SUBMITTAL
 06.06.2022

SITE PLAN -

Sheet Number

Project No: 21.70187.00

Original is 48 x 36. Do not scale contents of this drawing.

F 01



The Philips Lumec RoadFocus LED Cobra Head luminaires feature a sleek design that

provides seamless replacement of existing HID luminaires. RoadFocus is available

distributions, making it an outstanding solution for all types of roadway applications.

Includes Service Tag, Philips innovative way to provide assistance throughout the life

AST^{1,4} Pre-set driver for progressive start-up | API | Factory

CDMGS75 ¹⁴ 4 hrs. 75% reduction PH8/480 ¹ Twist-lock Photoelectric Cell, 480VAC

OTL¹⁴ Pre-set driver to signal end of life of PH9 Shorting cap

4. Dimming choices: Select either DMG, DALI or one of the CDMG options.

6. FAWS not available with CDMG options, DALI or CLO.

7. FAWS table accuracy s/- 15% on these models

FAWS⁶ Field adjustable

PH8/347¹ Twist-lock Photoelectric Cell, 347VAC

HS House Side Shield, shield, **WH** White

CDMGE25^{1,4} 8 hrs. 25% reduction

CDMGE50 1.4 8 hrs. 50% reduction

CDMGE75^{1,4} 8 hrs. 75% reduction

CDMGM25 1,4 6 hrs. 25% reduction

CDMGM50^{1,4} 6 hrs. 50% reduction CDMGM75 1,4 6 hrs. 75% reduction

CDMGS25^{1,4} 4 hrs. 25% reduction

CDMGS50^{1,4} 4 hrs. 50% reduction

DALI 1.4 Digitally addressable lighting interface

depreciation

2. Not available with HS option.

3. Use of photoelectric cell or shorting cap is required to ensure proper illumination.

5. Please note this integrated feature come standard with RoadFocus.

7. FAWS table accuracy +/- 15% on these models.

8. Not available with driver options, AST, CLO, CDMG, DALI, OTL.

in three sizes, offers multiple lumen packages, and a complete array of optical

of the product.

LUMEC

Roadway

RFL (large)

oadFocus

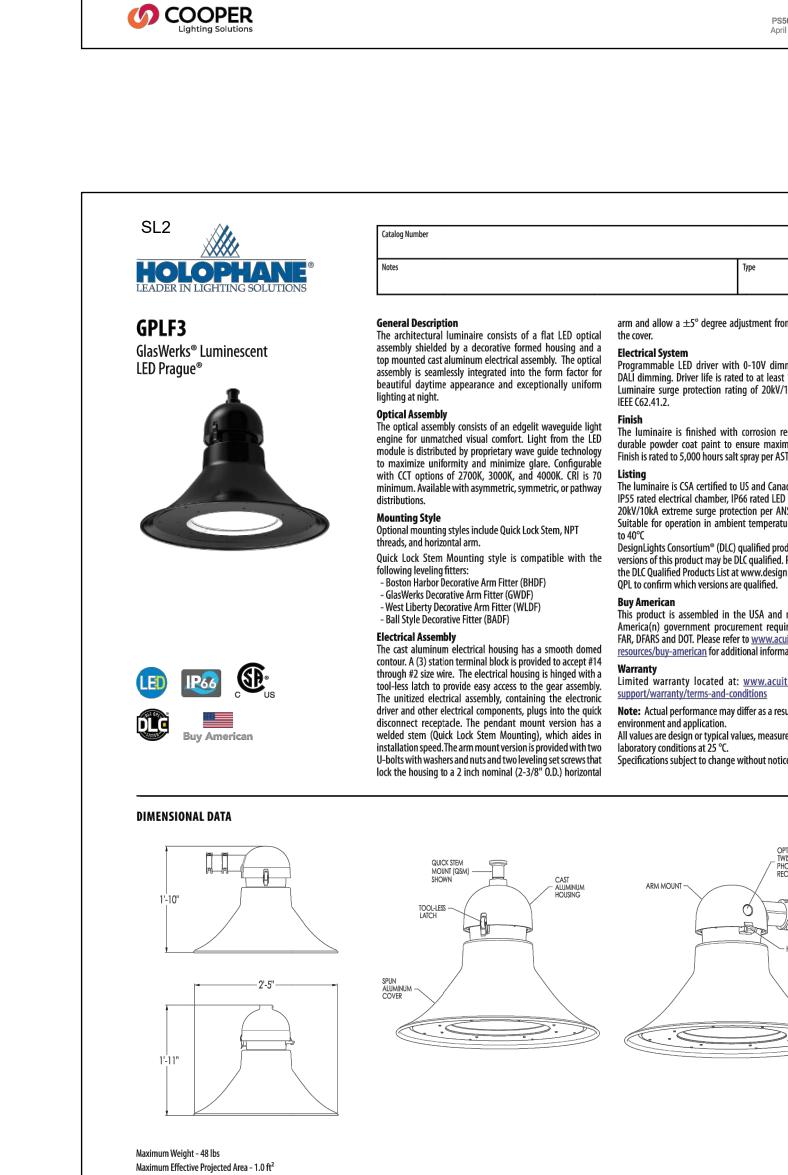
3000K
145W64LED3K
90W80LED3K
135W80LED3K
180W80LED3K
160W96LED3K
190W112LED3K
241W112LED3K
335W96LED3K
350W112LED3K8

4000K

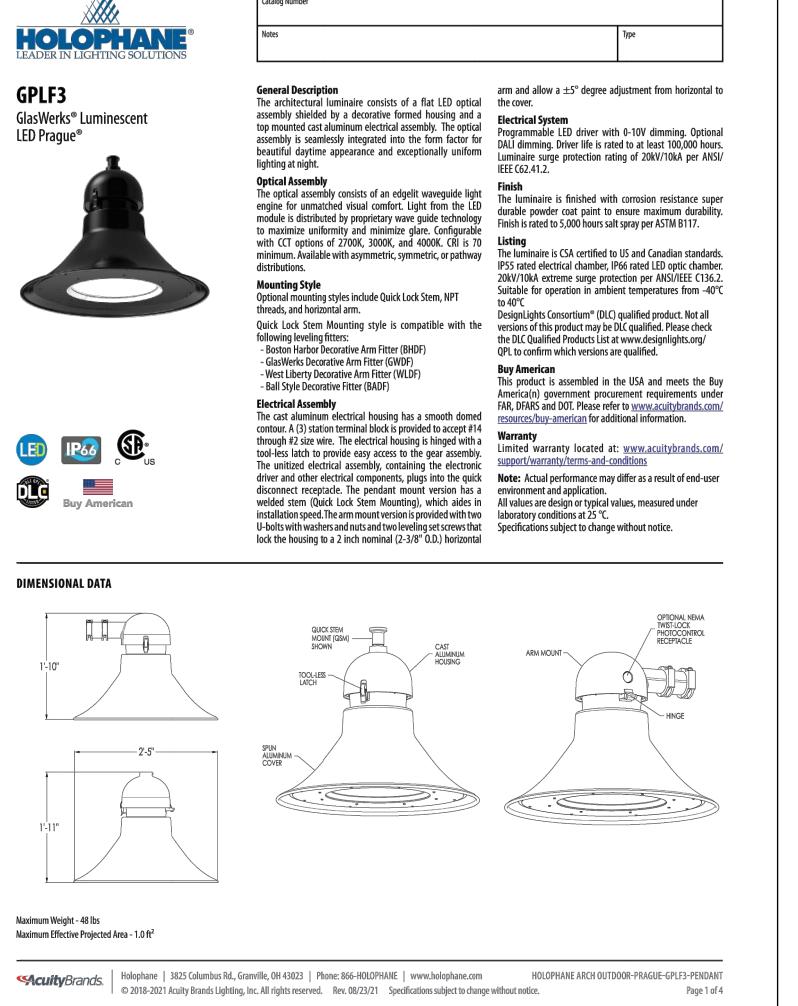
145W64LED4K
90W80LED4K
180W80LED4K
160W96LED4K
190W112LED4K
215W96LED4K
335W96LED4K
355W9112LED4K

347V and 480V not available.

RFL_Spec 09/17 page 1 of 5







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West Bottoms Redevelopment

Project

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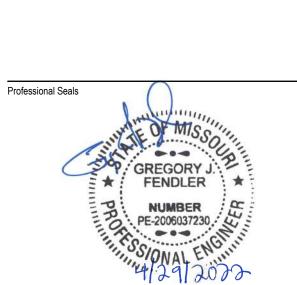
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01 UR RE-SUBMITTAL 02 UR RE-SUBMITTAL

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Sheet Title

ELECTRICAL -

Original is 48 x 36. Do not scale contents of this drawing. Sheet Number